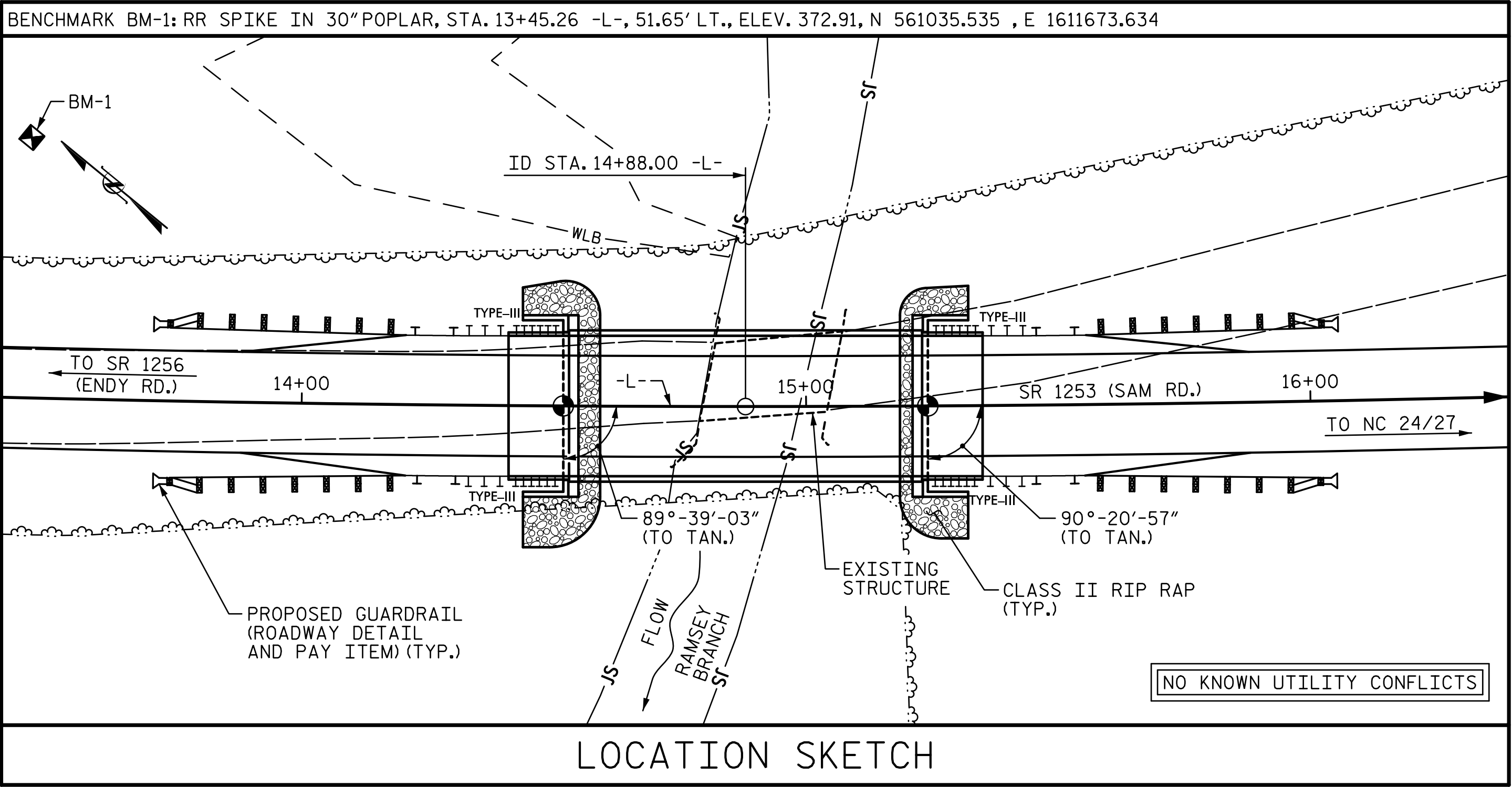


R:\Structures\ustation\401_003_B-5800_SMU_GD02_002_830160.dgn 8/2/2017 12:33:36 PM Jones



| TOTAL BILL OF MATERIAL | | | | | | | | | | |
|------------------------|-------------------------------|-----------------------------------|------------------|-----------------------|-------------------|---|------------------------|----------|-------------------|-----------------------|
| | REMOVAL OF EXISTING STRUCTURE | UNCLASSIFIED STRUCTURE EXCAVATION | CLASS A CONCRETE | BRIDGE APPROACH SLABS | REINFORCING STEEL | PILE DRIVING EQUIPMENT SETUP FOR HP 14 X 73 STEEL PILES | HP 14 X 73 STEEL PILES | | STEEL PILE POINTS | PREDRILLING FOR PILES |
| | LUMP SUM | LUMP SUM | CU. YDS. | LUMP SUM | LBS. | EA. | NO. | LIN. FT. | EA. | LIN. FT. |
| SUPERSTRUCTURE | | | | | | | | | | |
| | | | | | | | | | | |
| END BENT 1 | | | 21.7 | | 2,480 | 5 | 5 | 75.0 | 5 | 50.0 |
| END BENT 2 | | | 21.7 | | 2,480 | 5 | 5 | 75.0 | 5 | 50.0 |
| | | | | | | | | | | |
| TOTAL | LUMP SUM | LUMP SUM | 43.4 | LUMP SUM | 4,960 | 10 | 10 | 150.0 | 10 | 100.0 |

| TOTAL BILL OF MATERIAL (CONTINUED) | | | | | | | |
|------------------------------------|---|--------------------------------------|-------------------------------|-------------------------|---|----------|------------------------|
| | VERTICAL CONCRETE BARRIER RAIL | RIP RAP CLASS II (2'-0" THICK) | GEOTEXTILE FOR DRAINAGE | ELASTOMERIC BEARINGS | 3'-0" x 2'-0" PRESTRESSED CONCRETE CORED SLAB UNIT | | ASBESTOS ASSESSMENT |
| | LIN. FT. | TONS | SQ. YDS. | LUMP SUM | NO. | LIN. FT. | LUMP SUM |
| SUPERSTRUCTURE | 140.3 | | | | 10 | 700.0 | |
| | | | | | | | |
| END BENT 1 | | 70 | 75 | | | | |
| END BENT 2 | | 55 | 60 | | | | |
| | | | | | | | |
| TOTAL | 140.3 | 125 | 135 | LUMP SUM | 10 | 700.0 | LUMP SUM |

HYDRAULIC DATA

DESIGN DISCHARGE: 1,300 CFS
FREQUENCY OF DESIGN FLOOD: 25 YRS.
DESIGN HIGH WATER ELEVATION: 368.2
DRAINAGE AREA: 4.3 SQ. MI.
BASE DISCHARGE (Q100): 1,900 CFS
BASE HIGH WATER ELEVATION: 369.58

OVERTOPPING DATA

OVERTOPPING DISCHARGE: 2,800 CFS
FREQUENCY OF OVERTOPPING: 500+ YRS.
OVERTOPPING FLOOD ELEVATION: 370.4

GENERAL NOTES

ASSUMED LIVE LOAD = HL-93 OR ALTERNATE LOADING.

THIS BRIDGE HAS BEEN DESIGNED IN ACCORDANCE WITH THE REQUIREMENTS OF THE AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS.

THIS BRIDGE IS LOCATED IN SEISMIC ZONE 1.

FOR OTHER DESIGN DATA AND GENERAL NOTES, SEE "STANDARD NOTES" SHEET.

FOR EROSION CONTROL MEASURES, SEE EROSION CONTROL PLANS.

THE EXISTING STRUCTURE CONSISTING OF (1) 25'-6"± TIMBER DECK ON STEEL I-BEAMS SPAN WITH A CLEAR ROADWAY OF 17.23' ON TIMBER CAPS, POSTS AND SILLS AND LOCATED AT THE PROPOSED STRUCTURE SHALL BE REMOVED. FOR PARTIAL REMOVAL OF SILLS, SEE SHEET 1 OF 2.

REMOVAL OF THE EXISTING BRIDGE SHALL BE PERFORMED SO AS NOT TO ALLOW DEBRIS TO FALL INTO THE WATER. THE CONTRACTOR SHALL REMOVE THE BRIDGE AND SUBMIT PLANS FOR DEMOLITION IN ACCORDANCE WITH ARTICLE 402-2 OF THE STANDARD SPECIFICATIONS.

INASMUCH AS THE PAINT SYSTEM ON THE EXISTING STRUCTURAL STEEL CONTAINS LEAD, THE CONTRACTOR'S ATTENTION IS DIRECTED TO ARTICLE 107-1 OF THE STANDARD SPECIFICATIONS. ANY COSTS RESULTING FROM COMPLIANCE WITH APPLICABLE STATE OR FEDERAL REGULATIONS PERTAINING TO HANDLING OF MATERIALS CONTAINING LEAD BASED PAINT SHALL BE INCLUDED IN THE BID PRICE FOR "REMOVAL OF EXISTING STRUCTURE".

THE MATERIAL SHOWN IN THE CROSS-HATCHED AREA (ON SHEET 1 OF 2) SHALL BE EXCAVATED FOR A DISTANCE FROM THE CENTERLINE OF ROADWAY OF 32'± (LEFT) AND 45'± (RIGHT) AT END BENT 1 AND 25'± (LEFT AND RIGHT) AT END BENT 2, AND TO AN ELEVATION OF 364.0 AT END BENTS 1 AND 2 AS DIRECTED BY THE ENGINEER. THIS WORK WILL BE PAID FOR AT THE CONTRACT LUMP SUM PRICE FOR UNCLASSIFIED STRUCTURE EXCAVATION. SEE SECTION 412 OF THE STANDARD SPECIFICATIONS.

THIS STRUCTURE HAS BEEN DESIGNED IN ACCORDANCE WITH "HEC 18 - EVALUATING SCOUR AT BRIDGES".

THE SUBSTRUCTURE OF THE EXISTING BRIDGE INDICATED ON THE PLANS IS FROM THE BEST INFORMATION AVAILABLE. SINCE THIS INFORMATION IS SHOWN FOR THE CONVENIENCE OF THE CONTRACTOR, THE CONTRACTOR SHALL HAVE NO CLAIM WHATSOEVER AGAINST THE DEPARTMENT OF TRANSPORTATION FOR ANY DELAYS OR ADDITIONAL COST INCURRED BASED ON DIFFERENCES BETWEEN THE EXISTING BRIDGE SUBSTRUCTURE SHOWN ON THE PLANS AND THE ACTUAL CONDITIONS AT THE PROJECT SITE.

FOR SUBMITTAL OF WORKING DRAWINGS, SEE SPECIAL PROVISIONS.

FOR FALSEWORK AND FORMWORK, SEE SPECIAL PROVISIONS.

FOR CRANE SAFETY, SEE SPECIAL PROVISIONS.

FOR GROUT FOR STRUCTURES, SEE SPECIAL PROVISIONS.

ASPHALT WEARING SURFACE IS INCLUDED IN ROADWAY QUANTITY ON ROADWAY PLANS.

FOR ASBESTOS ASSESSMENT FOR BRIDGE DEMOLITION AND RENOVATION ACTIVITIES, SEE SPECIAL PROVISIONS.

FOUNDATION NOTES

FOR PILES, SEE GEOTECHNICAL SPECIAL PROVISIONS AND SECTION 450 OF THE STANDARD SPECIFICATIONS.

PILES AT END BENT 1 ARE DESIGNED FOR A FACTORED RESISTANCE OF 100 TONS PER PILE.

DRIVE PILES AT END BENT 1 TO A REQUIRED DRIVING RESISTANCE OF 167 TONS PER PILE.

PREDRILLING FOR PILES IS REQUIRED AT END BENT 1. PREDRILL PILE LOCATIONS TO AN ELEVATION NO LOWER THAN 355 FT WITH EQUIPMENT THAT WILL RESULT IN A MAXIMUM PREDRILLING DIAMETER OF 14". FOR PREDRILLING FOR PILES, SEE SECTION 450 OF THE STANDARD SPECIFICATIONS.

PILES AT END BENT 2 ARE DESIGNED FOR A FACTORED RESISTANCE OF 100 TONS PER PILE.

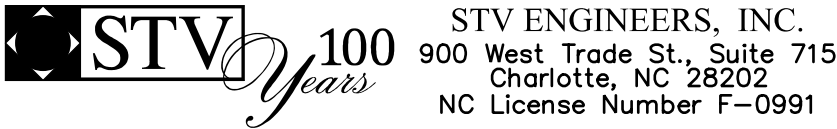
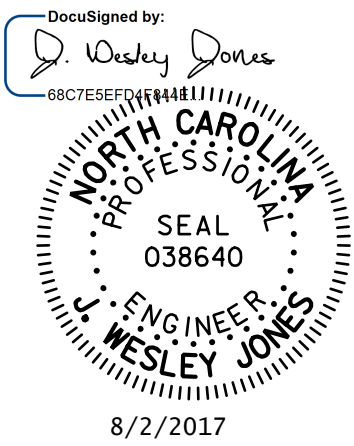
DRIVE PILES AT END BENT 2 TO A REQUIRED DRIVING RESISTANCE OF 167 TONS PER PILE.

PREDRILLING FOR PILES IS REQUIRED AT END BENT 2. PREDRILL PILE LOCATIONS TO AN ELEVATION NO LOWER THAN 353.6 FT WITH EQUIPMENT THAT WILL RESULT IN A MAXIMUM PREDRILLING DIAMETER OF 14". FOR PREDRILLING FOR PILES, SEE SECTION 450 OF THE STANDARD SPECIFICATIONS.

STEEL H-PILE POINTS ARE REQUIRED FOR STEEL H-PILES AT END BENT 1 AND END BENT 2. FOR STEEL PILE POINTS, SEE SECTION 450 OF THE STANDARD SPECIFICATIONS.

PROJECT NO. B-5800
STANLEY COUNTY
STATION: 14+88.00 -L-

SHEET 2 OF 2



DOCUMENT NOT CONSIDERED
FINAL UNLESS ALL
SIGNATURES COMPLETED

DRAWN BY : ATH DATE : 7-16
CHECKED BY : JWJ DATE : 1-17
DESIGN ENGINEER OF RECORD : JWJ DATE : 5-17

STATE OF NORTH CAROLINA
DEPARTMENT OF TRANSPORTATION
RALEIGH

GENERAL DRAWING
FOR BRIDGE ON SR 1253
(SAM ROAD) OVER
RAMSEY BRANCH BETWEEN
SR 1256 AND NC 24/27

| REVISIONS | | | | | SHEET NO. |
|-----------|-----|-------|-----|-----|-----------------|
| NO. | BY: | DATE: | NO. | BY: | DATE: |
| 1 | | | 3 | | |
| 2 | | | 4 | | |
| | | | | | TOTAL SHEETS 13 |

LOAD FACTORS:

| DESIGN LOAD RATING FACTORS | LIMIT STATE | γ _{DC} | γ _{DW} |
|-------------------------------------|-------------|-----------------|-----------------|
| | STRENGTH I | 1.25 | 1.50 |
| | SERVICE III | 1.00 | 1.00 |

NOTES:

MINIMUM RATING FACTORS ARE BASED ON THE STRENGTH I AND SERVICE III LIMIT STATES.

ALLOWABLE STRESSES FOR SERVICE III LIMIT STATE ARE AS REQUIRED FOR DESIGN.

COMMENTS:

1.
2.
3.
4.

CONTROLLING LOAD RATING

1 DESIGN LOAD RATING (HL-93)

2 DESIGN LOAD RATING (HS-20)

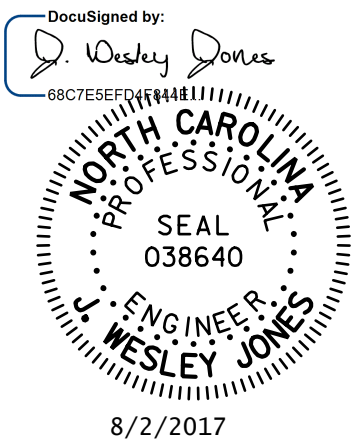
3 LEGAL LOAD RATING **

** SEE CHART FOR VEHICLE TYPE

GIRDER LOCATION

I - INTERIOR GIRDER
EL - EXTERIOR LEFT GIRDER
ER - EXTERIOR RIGHT GIRDER

PROJECT NO. B-5800
STANLY COUNTY
STATION: 14+88.00 -L-



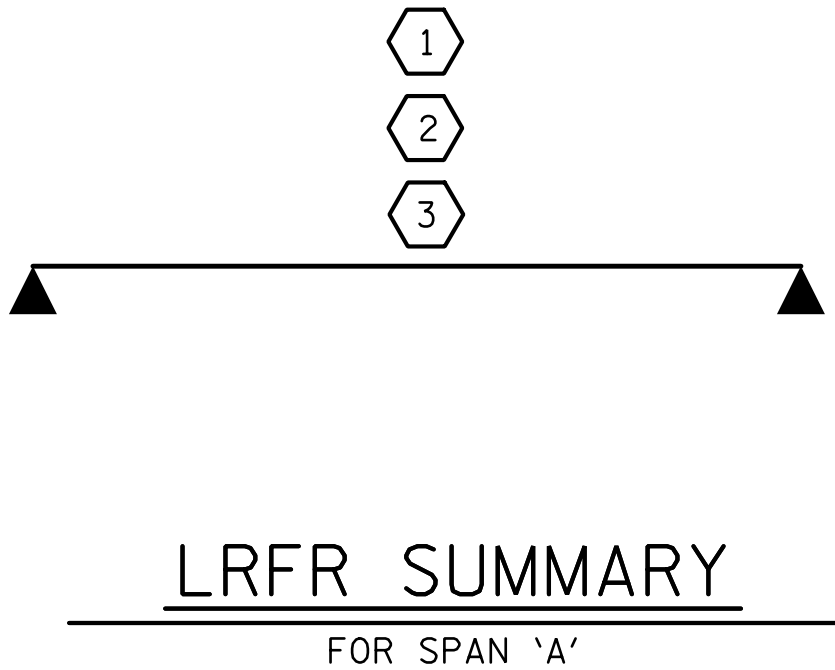
STV ENGINEERS, INC.
900 West Trade St., Suite 715
Charlotte, NC 28202
NC License Number F-0991

DOCUMENT NOT CONSIDERED
FINAL UNLESS ALL
SIGNATURES COMPLETED

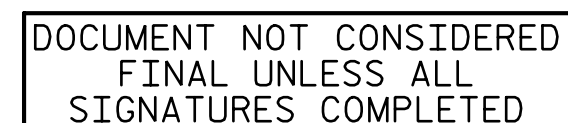
| REVISIONS | | | | | | SHEET NO. |
|-----------|-----|-------|-----|-----|-------|--------------|
| NO. | BY: | DATE: | NO. | BY: | DATE: | S-3 |
| 1 | | | 3 | | | TOTAL SHEETS |
| 2 | | | 4 | | | 13 |

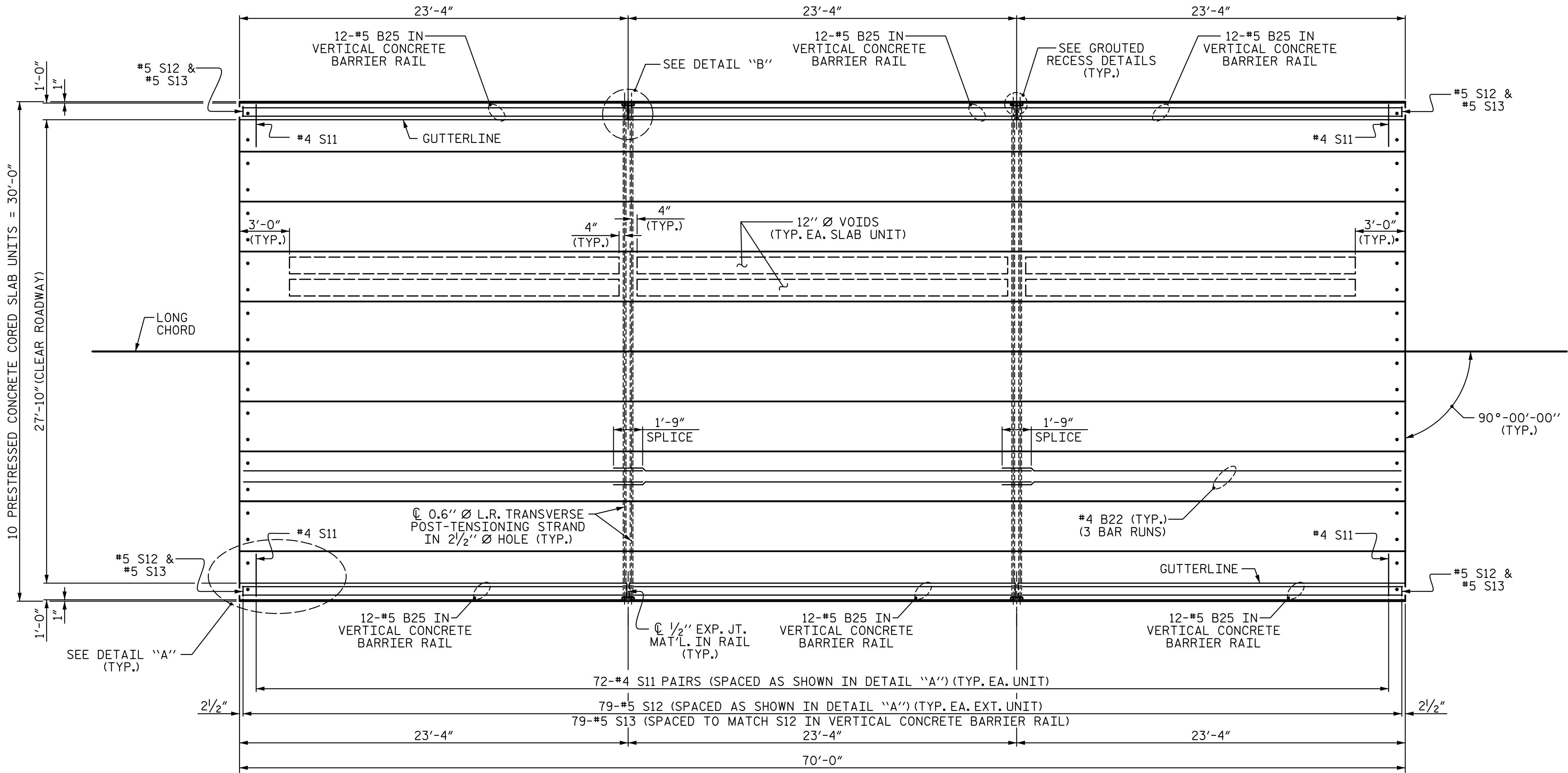
LOAD AND RESISTANCE FACTOR RATING (LRFD) SUMMARY FOR PRESTRESSED CONCRETE GIRDERS

| LEVEL | VEHICLE | WEIGHT (W) (TONS) | CONTROLLING LOAD RATING | MINIMUM RATING FACTORS (RF) | TONS = W X RF | STRENGTH I LIMIT STATE | | | | | | | | | | SERVICE III LIMIT STATE | | | | | | COMMENT NUMBER | | |
|--------------------------|------------|----------------------|----------------------------|-----------------------------------|---------------|------------------------|------------------------------|---------------|------|-----------------|---|------------------------------|---------------|------|-----------------|---|---------------------|------------------------------|---------------|------|-----------------|-------------------|---|--|
| | | | | | | LIVELOAD FACTORS | MOMENT | | | | | SHEAR | | | | | LIVELOAD FACTORS | MOMENT | | | | | | |
| | | | | | | | DISTRIBUTION FACTORS (DF) | RATING FACTOR | SPAN | GIRDER LOCATION | DISTANCE FROM LEFT END OF SPAN (ft) | DISTRIBUTION FACTORS (DF) | RATING FACTOR | SPAN | GIRDER LOCATION | DISTANCE FROM LEFT END OF SPAN (ft) | | DISTRIBUTION FACTORS (DF) | RATING FACTOR | SPAN | GIRDER LOCATION | | DISTANCE FROM LEFT END OF SPAN (ft) | |
| DESIGN LOAD RATING | HL-93(Inv) | N/A | 1 | 1.006 | -- | 1.75 | 0.273 | 1.03 | 70' | EL | 34.5 | 0.507 | 1.32 | 70' | EL | 6.9 | 0.80 | 0.273 | 1.01 | 70' | EL | 34.5 | | |
| | HL-93(0pr) | N/A | -- | 1.341 | -- | 1.35 | 0.273 | 1.34 | 70' | EL | 34.5 | 0.507 | 1.72 | 70' | EL | 6.9 | N/A | -- | -- | -- | -- | -- | | |
| | HS-20(Inv) | 36.000 | 2 | 1.306 | 47.02 | 1.75 | 0.273 | 1.34 | 70' | EL | 34.5 | 0.507 | 1.65 | 70' | EL | 6.9 | 0.80 | 0.273 | 1.31 | 70' | EL | 34.5 | | |
| | HS-20(0pr) | 36.000 | -- | 1.74 | 62.64 | 1.35 | 0.273 | 1.74 | 70' | EL | 34.5 | 0.507 | 2.14 | 70' | EL | 6.9 | N/A | -- | -- | -- | -- | -- | | |
| LEGAL LOAD RATING | SV | SNSH | 13.500 | -- | 2.917 | 39.379 | 1.4 | 0.273 | 3.75 | 70' | EL | 34.5 | 0.507 | 4.87 | 70' | EL | 6.9 | 0.80 | 0.273 | 2.92 | 70' | EL | 34.5 | |
| | | SNGARBS2 | 20.000 | -- | 2.187 | 43.741 | 1.4 | 0.273 | 2.81 | 70' | EL | 34.5 | 0.507 | 3.47 | 70' | EL | 6.9 | 0.80 | 0.273 | 2.19 | 70' | EL | 34.5 | |
| | | SNAGRIS2 | 22.000 | -- | 2.077 | 45.69 | 1.4 | 0.273 | 2.67 | 70' | EL | 34.5 | 0.507 | 3.23 | 70' | EL | 6.9 | 0.80 | 0.273 | 2.08 | 70' | EL | 34.5 | |
| | | SNCOTTS3 | 27.250 | -- | 1.452 | 39.565 | 1.4 | 0.273 | 1.87 | 70' | EL | 34.5 | 0.507 | 2.43 | 70' | EL | 6.9 | 0.80 | 0.273 | 1.45 | 70' | EL | 34.5 | |
| | | SNAGGRS4 | 34.925 | -- | 1.218 | 42.554 | 1.4 | 0.273 | 1.57 | 70' | EL | 34.5 | 0.507 | 2.03 | 70' | EL | 6.9 | 0.80 | 0.273 | 1.22 | 70' | EL | 34.5 | |
| | | SNS5A | 35.550 | -- | 1.191 | 42.346 | 1.4 | 0.273 | 1.53 | 70' | EL | 34.5 | 0.507 | 2.06 | 70' | EL | 6.9 | 0.80 | 0.273 | 1.19 | 70' | EL | 34.5 | |
| | | SNS6A | 39.950 | -- | 1.095 | 43.747 | 1.4 | 0.273 | 1.41 | 70' | EL | 34.5 | 0.507 | 1.88 | 70' | EL | 6.9 | 0.80 | 0.273 | 1.10 | 70' | EL | 34.5 | |
| | | SNS7B | 42.000 | -- | 1.043 | 43.801 | 1.4 | 0.273 | 1.34 | 70' | EL | 34.5 | 0.507 | 1.85 | 70' | EL | 6.9 | 0.80 | 0.273 | 1.04 | 70' | EL | 34.5 | |
| | TTST | TNAGRIT3 | 33.000 | -- | 1.336 | 44.087 | 1.4 | 0.273 | 1.72 | 70' | EL | 34.5 | 0.507 | 2.23 | 70' | EL | 6.9 | 0.80 | 0.273 | 1.34 | 70' | EL | 34.5 | |
| | | TNT4A | 33.075 | -- | 1.342 | 44.401 | 1.4 | 0.273 | 1.72 | 70' | EL | 34.5 | 0.507 | 2.17 | 70' | EL | 6.9 | 0.80 | 0.273 | 1.34 | 70' | EL | 34.5 | |
| | | TNT6A | 41.600 | -- | 1.1 | 45.746 | 1.4 | 0.273 | 1.41 | 70' | EL | 34.5 | 0.507 | 1.98 | 70' | EL | 6.9 | 0.80 | 0.273 | 1.10 | 70' | EL | 34.5 | |
| | | TNT7A | 42.000 | -- | 1.106 | 46.462 | 1.4 | 0.273 | 1.42 | 70' | EL | 34.5 | 0.507 | 1.94 | 70' | EL | 6.9 | 0.80 | 0.273 | 1.11 | 70' | EL | 34.5 | |
| | | TNT7B | 42.000 | -- | 1.147 | 48.18 | 1.4 | 0.273 | 1.47 | 70' | EL | 34.5 | 0.507 | 1.8 | 70' | EL | 6.9 | 0.80 | 0.273 | 1.15 | 70' | EL | 34.5 | |
| | | TNAGRIT4 | 43.000 | -- | 1.089 | 46.838 | 1.4 | 0.273 | 1.4 | 70' | EL | 34.5 | 0.507 | 1.74 | 70' | EL | 6.9 | 0.80 | 0.273 | 1.09 | 70' | EL | 34.5 | |
| TNAGT5A | | 45.000 | -- | 1.026 | 46.175 | 1.4 | 0.273 | 1.32 | 70' | EL | 34.5 | 0.507 | 1.74 | 70' | EL | 6.9 | 0.80 | 0.273 | 1.03 | 70' | EL | 34.5 | | |
| TNAGT5B | | 45.000 | 3 | 1.013 | 45.579 | 1.4 | 0.273 | 1.3 | 70' | EL | 34.5 | 0.507 | 1.66 | 70' | EL | 6.9 | 0.80 | 0.273 | 1.01 | 70' | EL | 34.5 | | |

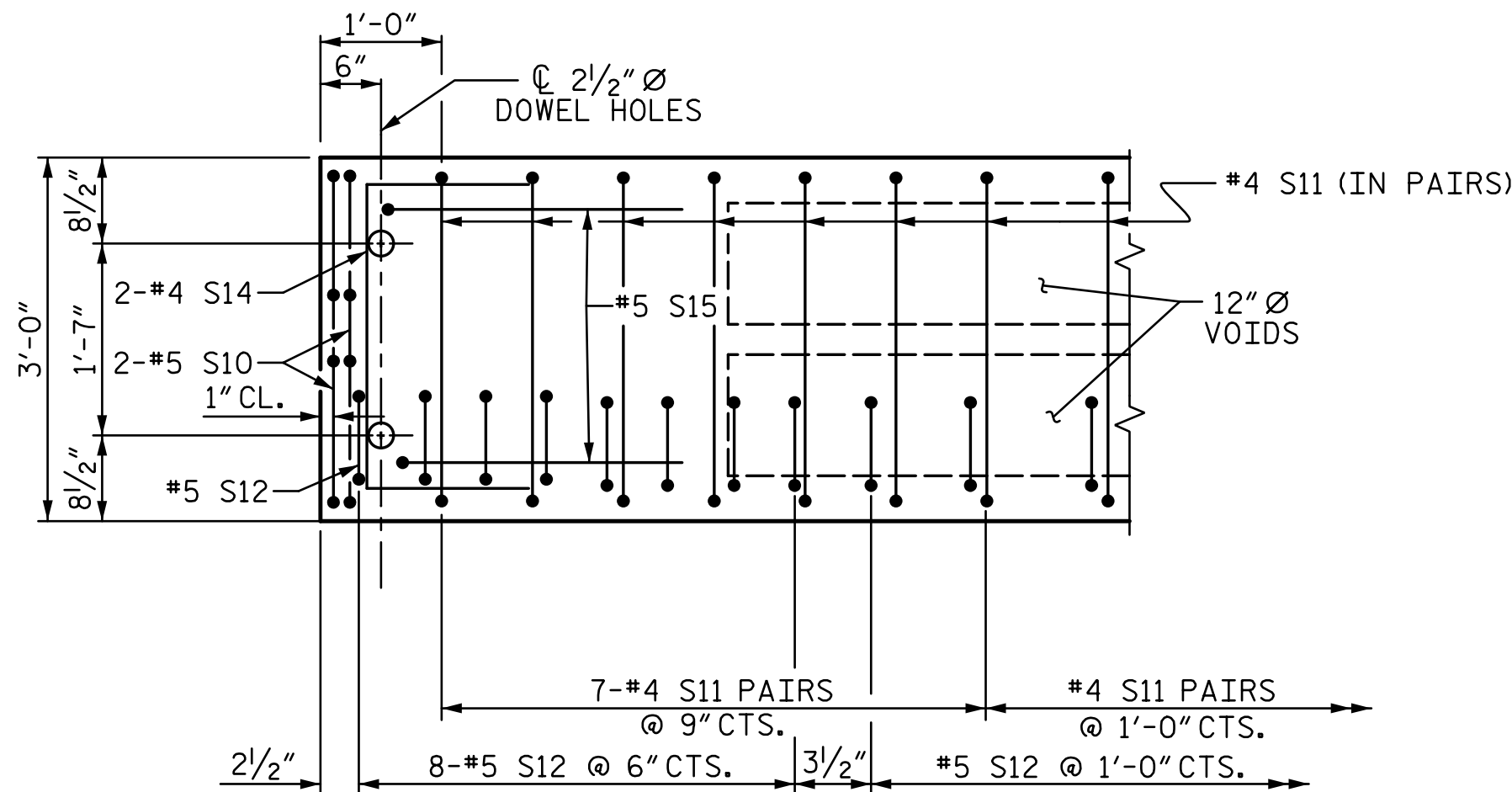


| | |
|--|--------------------|
| DRAWN BY : <u>ATH</u> | DATE : <u>7-16</u> |
| CHECKED BY : <u>JWJ</u> | DATE : <u>2-17</u> |
| DESIGN ENGINEER OF RECORD : <u>JWJ</u> | DATE : <u>5-17</u> |
| DRAWN BY : CVC 6/10 | |
| CHECKED BY : DNS 6/10 | |



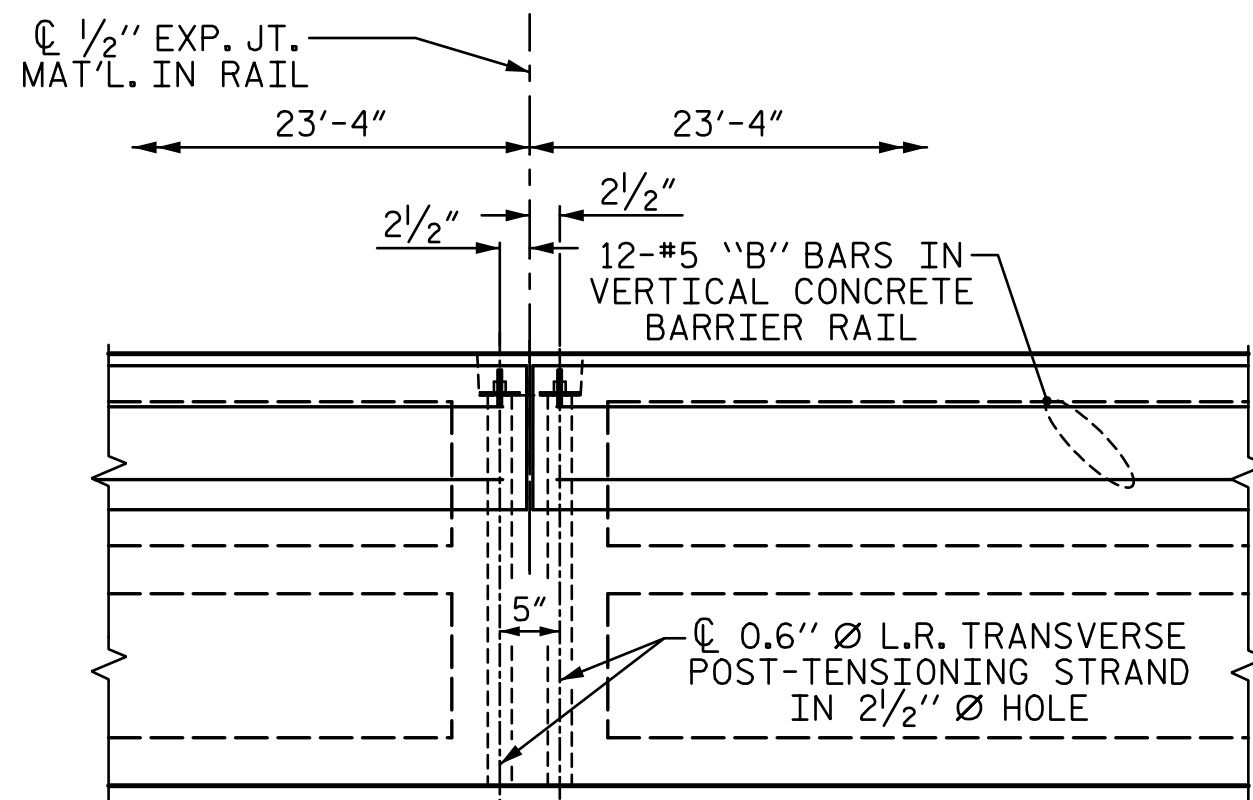


PLAN OF UNIT



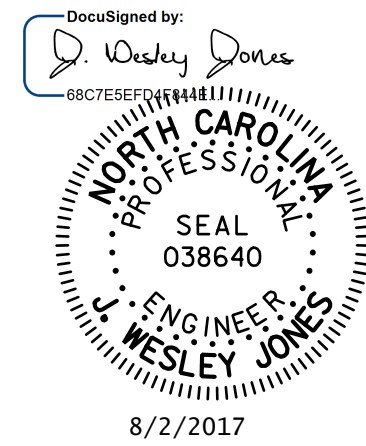
DETAIL "A"

(TYPICAL EACH END OF UNIT)
NOTE: EXTERIOR UNIT SHOWN - INTERIOR UNIT SIMILAR EXCEPT OMIT #5 S12 BARS.



DETAIL "B"

#4 S11 BARS MAY BE SHIFTED AS NECESSARY TO MAINTAIN 1" CLEAR TO GROUTED RECESS AND 2 1/2" Ø TRANSVERSE POST-TENSIONING STRAND HOLES



STV ENGINEERS, INC.
900 West Trade St., Suite 715
Charlotte, NC 28202
NC License Number F-0991

DOCUMENT NOT CONSIDERED
FINAL UNLESS ALL
SIGNATURES COMPLETED

PROJECT NO. **B-5800**

STANLY COUNTY

STATION: **14+88.00 -L-**

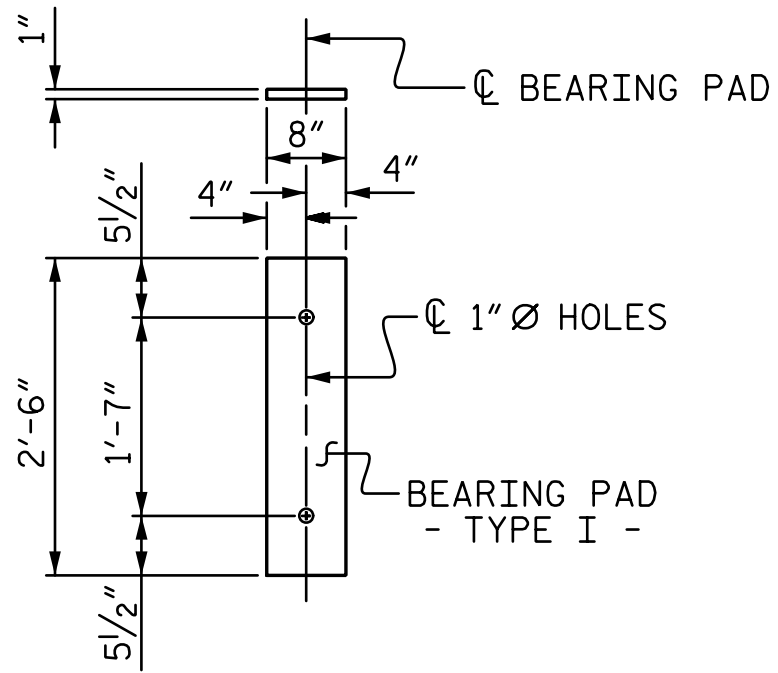
SHEET 2 OF 3

STATE OF NORTH CAROLINA
DEPARTMENT OF TRANSPORTATION
RALEIGH

PLAN OF 70' UNIT
27'-10" CLEAR ROADWAY
90° SKEW

| REVISIONS | | | | | SHEET NO. |
|-----------|-----|-------|-----|-----|-----------------|
| NO. | BY: | DATE: | NO. | BY: | DATE: |
| 1 | | | 3 | | |
| 2 | | | 4 | | |
| | | | | | S-5 |
| | | | | | TOTAL SHEETS 13 |

STD. NO. 24PCS_30_90S_70L



FIXED END
(TYPE I - 20 REQ'D)

ELASTOMERIC BEARING DETAILS

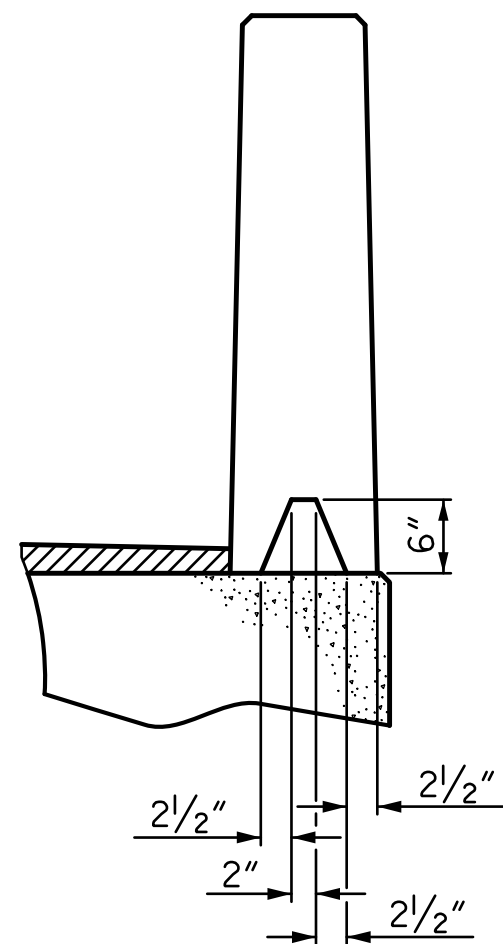
ELASTOMER IN ALL BEARINGS SHALL BE 60 DUROMETER HARDNESS.

| DEAD LOAD DEFLECTION AND CAMBER | |
|--|-------------------------------------|
| 70' CORED SLAB UNIT | 3'-0" x 2'-0" 0.6" Ø L.R. STRAND |
| CAMBER (SLAB ALONE IN PLACE) | 2 1/4" ↑ |
| DEFLECTION DUE TO SUPERIMPOSED DEAD LOAD** | 3/4" ↓ |
| FINAL CAMBER | 1 1/2" ↑ |

** INCLUDES FUTURE WEARING SURFACE

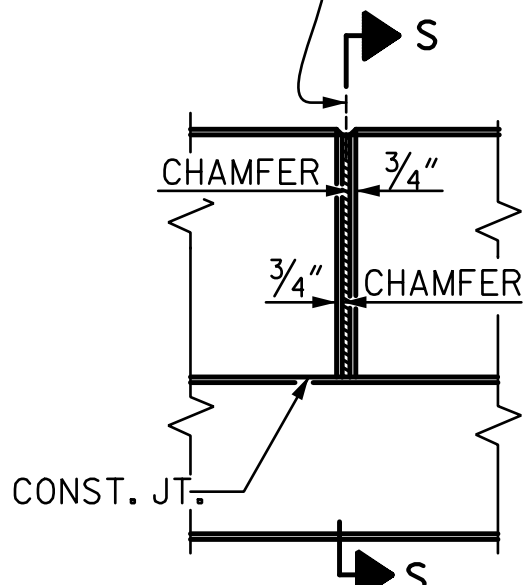
| CORED SLABS REQUIRED | | | |
|----------------------|--------|--------|--------------|
| 70' UNIT | NUMBER | LENGTH | TOTAL LENGTH |
| EXTERIOR C.S. | 2 | 70'-0" | 140'-0" |
| INTERIOR C.S. | 8 | 70'-0" | 560'-0" |
| TOTAL | 10 | | 700'-0" |

| GRADE 270 STRANDS | |
|---------------------------------------|-------------|
| AREA (SQUARE INCHES) | 0.6" Ø L.R. |
| ULTIMATE STRENGTH (LBS. PER STRAND) | 58,600 |
| APPLIED PRESTRESS (LBS. PER STRAND) | 43,950 |



SECTION S-S
AT DAM IN OPEN JOINT
(THIS IS TO BE USED ONLY
WHEN SLIP FORM IS USED)

Ø 1/2" EXP. JT. MAT'L HELD IN
PLACE WITH GALVANIZED NAILS.
(NOTE: OMIT EXP. JT. MAT'L
WHEN SLIP FORM IS USED)

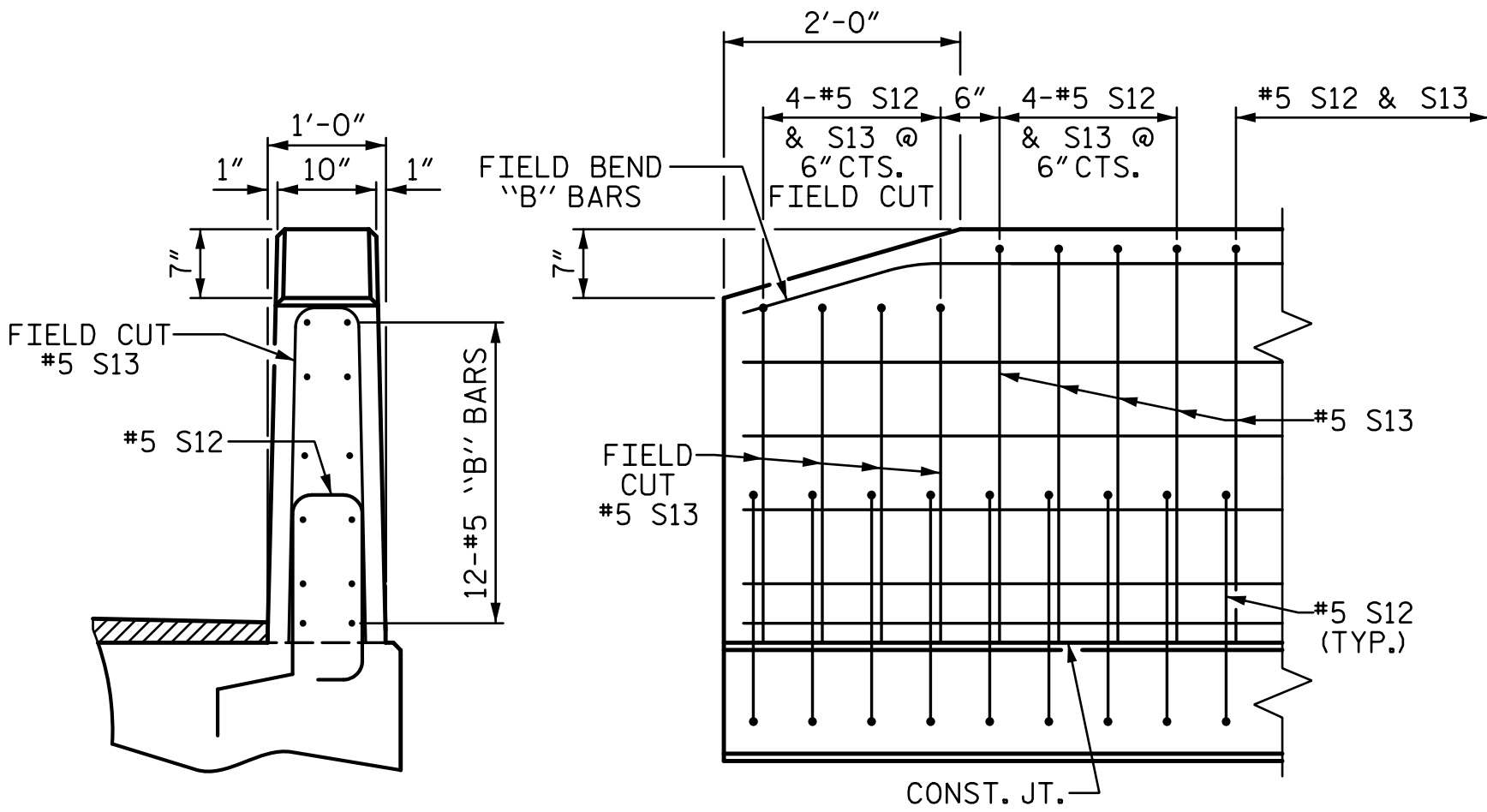


ELEVATION AT EXPANSION JOINTS

VERTICAL CONCRETE BARRIER RAIL DETAILS

| BILL OF MATERIAL FOR VERTICAL CONCRETE BARRIER RAIL | | | | | | |
|---|---------------------------------|-----------|---------|------|---------|--------|
| BAR | BARS PER PAIR OF EXTERIOR UNITS | TOTAL NO. | SIZE | TYPE | LENGTH | WEIGHT |
| 70' UNIT | | | | | | |
| *B25 | 72 | 72 | #5 | STR | 22'-11" | 1721 |
| *S13 | 158 | 158 | #5 | 2 | 7'-2" | 1181 |
| *EPOXY COATED REINFORCING STEEL | | | LBS. | | 2902 | |
| CLASS AA CONCRETE | | | CU.YDS. | | 18.7 | |
| TOTAL VERTICAL CONCRETE BARRIER RAIL | | | LN. FT. | | 140.25 | |

| GUTTERLINE ASPHALT THICKNESS & RAIL HEIGHT | | |
|--|---|---------------------------|
| | ASPHALT OVERLAY THICKNESS @ MID-SPAN | RAIL HEIGHT @ MID-SPAN |
| 70' UNITS | 1 5/8" | 3'-7 5/8" |

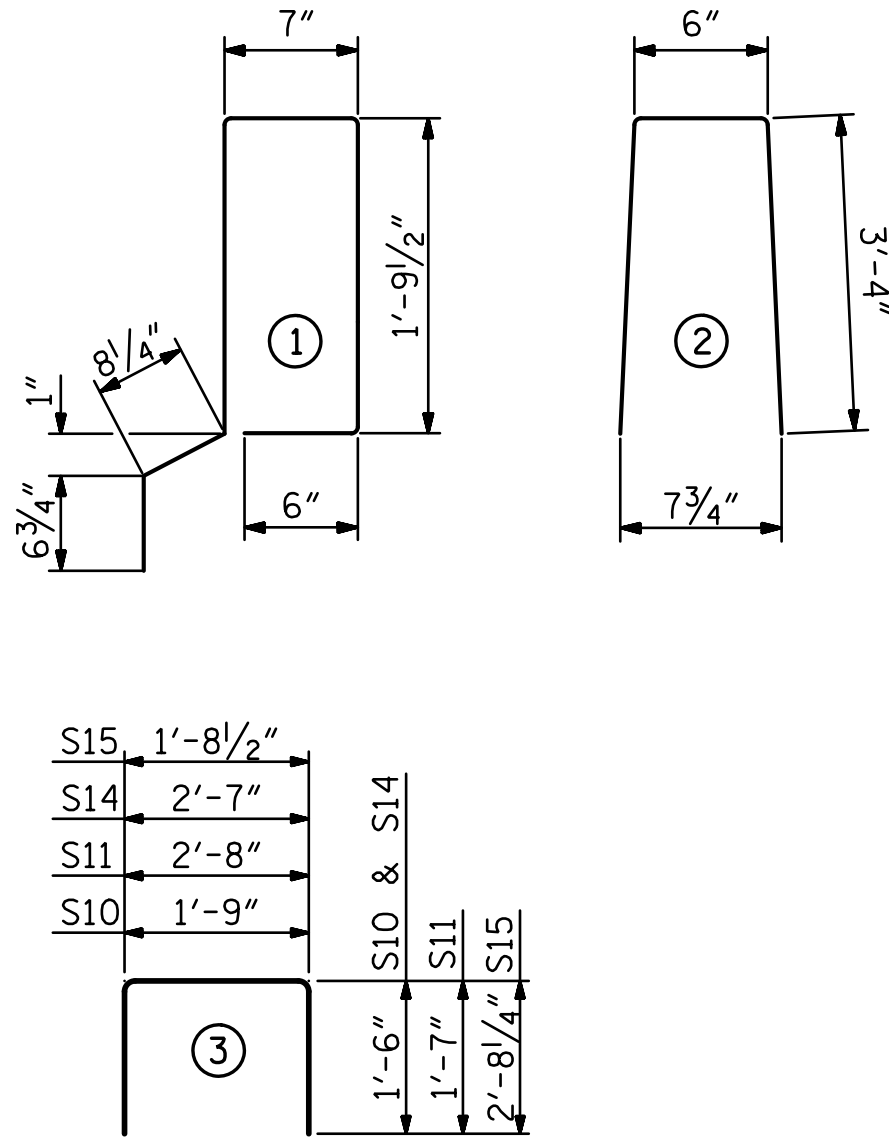


END VIEW

SIDE VIEW

END OF RAIL DETAILS

BAR TYPES



ALL BAR DIMENSIONS ARE OUT TO OUT

NOTES

ALL PRESTRESSING STRANDS SHALL BE 7-WIRE LOW RELAXATION GRADE 270 STRANDS AND SHALL CONFORM TO AASHTO M203 EXCEPT FOR SAMPLING REQUIREMENTS WHICH SHALL BE IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS.

ALL REINFORCING STEEL CAST WITH THE CORED SLAB SECTIONS SHALL BE GRADE 60 AND SHALL BE INCLUDED IN THE UNIT PRICE BID FOR PRESTRESSED CONCRETE CORED SLABS.

RECESSES FOR TRANSVERSE STRANDS SHALL BE GROUTED AFTER THE TENSIONING OF THE STRANDS.

THE 2 1/2" Ø DOWEL HOLES AT FIXED ENDS OF SLAB SECTIONS SHALL BE FILLED WITH NON-SHRINK GROUT.

THE BACKER RODS SHALL CONFORM TO THE REQUIREMENTS OF TYPE M BOND BREAKER. SEE SECTION 1028 OF THE STANDARD SPECIFICATIONS.

WHEN CORED SLABS ARE CAST, AN INTERNAL HOLD-DOWN SYSTEM SHALL BE EMPLOYED TO PREVENT VOIDS FROM RISING OR MOVING SIDWAYS. AT LEAST SIX WEEKS PRIOR TO CASTING CORED SLABS, THE CONTRACTOR SHALL SUBMIT TO THE ENGINEER FOR REVIEW AND COMMENT, DETAILED DRAWINGS OF THE PROPOSED HOLD-DOWN SYSTEM. IN ADDITION TO STRUCTURAL DETAILS, LOCATION AND SPACING OF THE HOLD-DOWNS SHALL BE INDICATED.

THE TRANSFER OF LOAD FROM THE ANCHORAGES TO THE CORED SLAB UNIT SHALL BE DONE WHEN THE CONCRETE HAS REACHED A COMPRESSIVE STRENGTH OF NOT LESS THAN THE REQUIRED STRENGTH SHOWN IN THE "CONCRETE RELEASE STRENGTH" TABLE.

ALL REINFORCING STEEL IN VERTICAL CONCRETE BARRIER RAILS SHALL BE EPOXY COATED.

PRESTRESSING STRANDS SHALL BE CUT FLUSH WITH THE CORED SLAB UNIT ENDS.

APPLY EPOXY PROTECTIVE COATING TO CORED SLAB UNIT ENDS.

GROOVED CONTRACTION JOINTS, 1/2" IN DEPTH, SHALL BE TOOLED IN ALL EXPOSED FACES OF THE BARRIER RAIL AND IN ACCORDANCE WITH ARTICLE 825-10(B) OF THE STANDARD SPECIFICATIONS. A CONTRACTION JOINT SHALL BE LOCATED AT EACH THIRD POINT BETWEEN BARRIER RAIL EXPANSION JOINTS. ONLY ONE CONTRACTION JOINT IS REQUIRED AT MIDPOINT OF BARRIER RAIL SEGMENTS LESS THAN 20 FEET IN LENGTH AND NO CONTRACTION JOINTS ARE REQUIRED FOR THOSE SEGMENTS LESS THAN 10 FEET IN LENGTH.

FLAME CUTTING OF THE TRANSVERSE POST-TENSIONING STRAND IS NOT ALLOWED.

MAINTAIN A SYMMETRIC TENSION FORCE BETWEEN EACH PAIR OF TRANSVERSE POST TENSIONING STRANDS IN THE DIAPHRAGM.

THE #4 S11 STIRRUPS MAY BE SHIFTED AS NECESSARY TO MAINTAIN 1" CLEAR TO THE GROUTED RECESS.

FOR GROUT FOR STRUCTURES, SEE SPECIAL PROVISIONS.

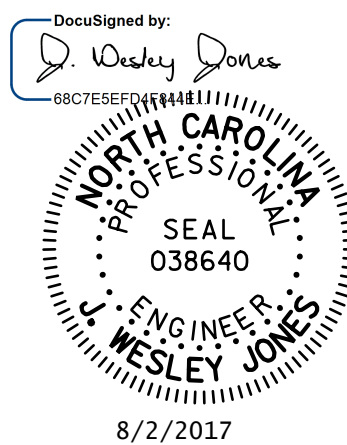
THE PERMITTED THREADED INSERTS ARE DETAILED AS AN OPTION FOR THE CONTRACTOR TO ATTACH FALSEWORK AND FORMWORK DURING CONSTRUCTION.

THE PERMITTED THREADED INSERTS IN THE EXTERIOR UNITS SHALL BE SIZED BY THE CONTRACTOR, SPACED AT 4'-0" CENTERS AND GALVANIZED IN ACCORDANCE WITH SECTION 1076 OF THE STANDARD SPECIFICATIONS. STAINLESS STEEL THREADED INSERTS MAY BE USED AS AN ALTERNATE.

THE PERMITTED THREADED INSERTS SHALL BE GROUTED BY THE CONTRACTOR IMMEDIATELY FOLLOWING REMOVAL OF THE FALSEWORK.

THE COST OF THE PERMITTED THREADED INSERTS SHALL BE INCLUDED IN THE PRICE BID FOR THE PRECAST UNITS.

| CONCRETE RELEASE STRENGTH | |
|---------------------------|------|
| UNIT | PSI |
| 70' UNITS | 5500 |



PROJECT NO. **B-5800**

STANLY COUNTY

STATION: **14+88.00 -L-**

SHEET 3 OF 3

STATE OF NORTH CAROLINA
DEPARTMENT OF TRANSPORTATION
RALEIGH

STANDARD
3'-0" X 2'-0"
PRESTRESSED CONCRETE
CORED SLAB UNIT

| REVISIONS | | | | SHEET NO. | |
|-----------|-----|-------|-----|--------------|-------|
| NO. | BY: | DATE: | NO. | BY: | DATE: |
| 1 | | | 3 | | |
| 2 | | | 4 | | |
| | | | | TOTAL SHEETS | 13 |

STD. NO. 24PCS3_30_905

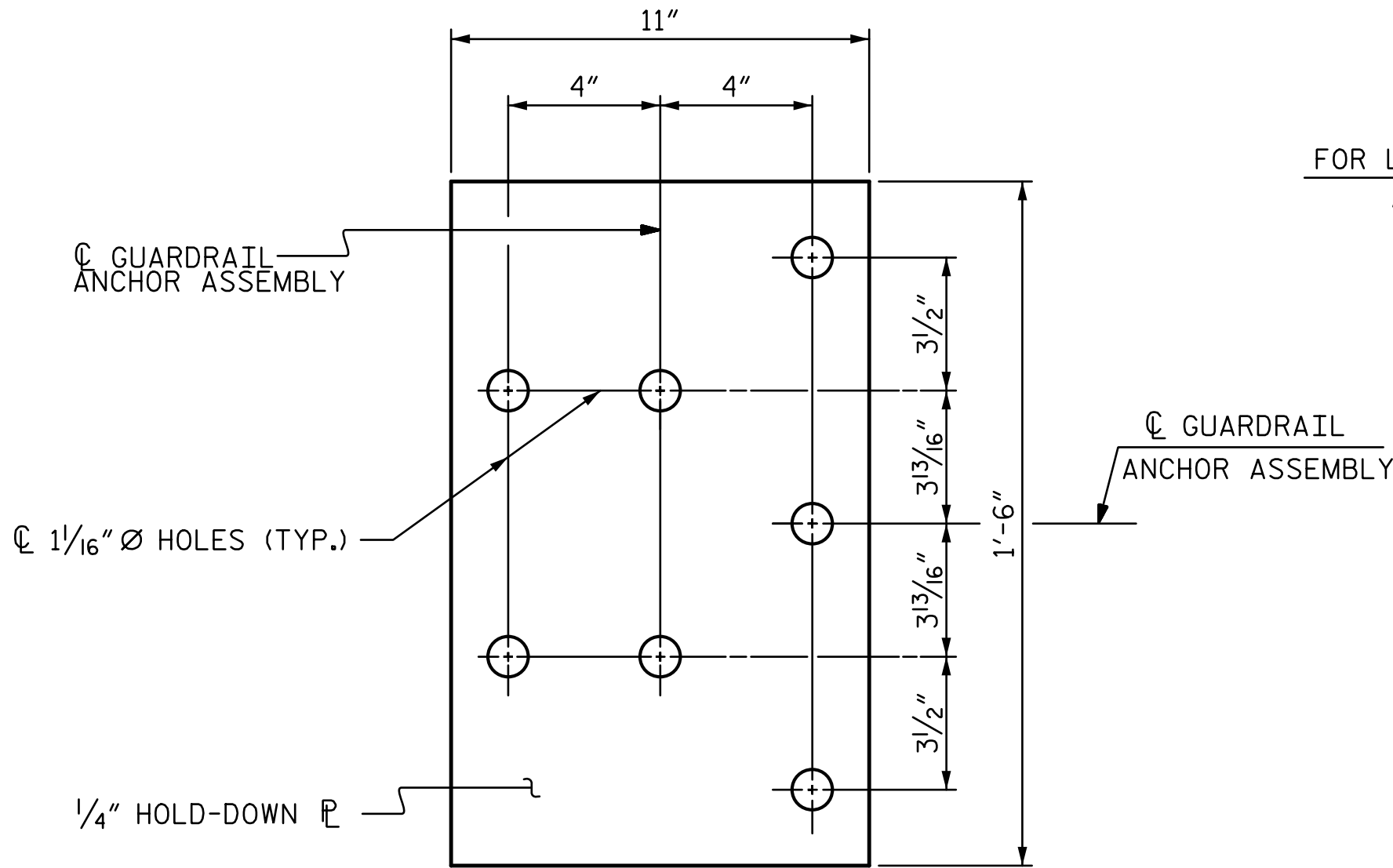
| | |
|--|--------------------|
| DRAWN BY : ATH | DATE : 7-16 |
| CHECKED BY : JWJ | DATE : 2-17 |
| DESIGN ENGINEER OF RECORD : JWJ | DATE : 5-17 |
| DRAWN BY : MAA | 6/10 |
| CHECKED BY : MKT | 7/10 |
| REV. 11/14 | MAA/TMG |



STV ENGINEERS, INC.
900 West Trade St., Suite 715
Charlotte, NC 28202
NC License Number F-0991

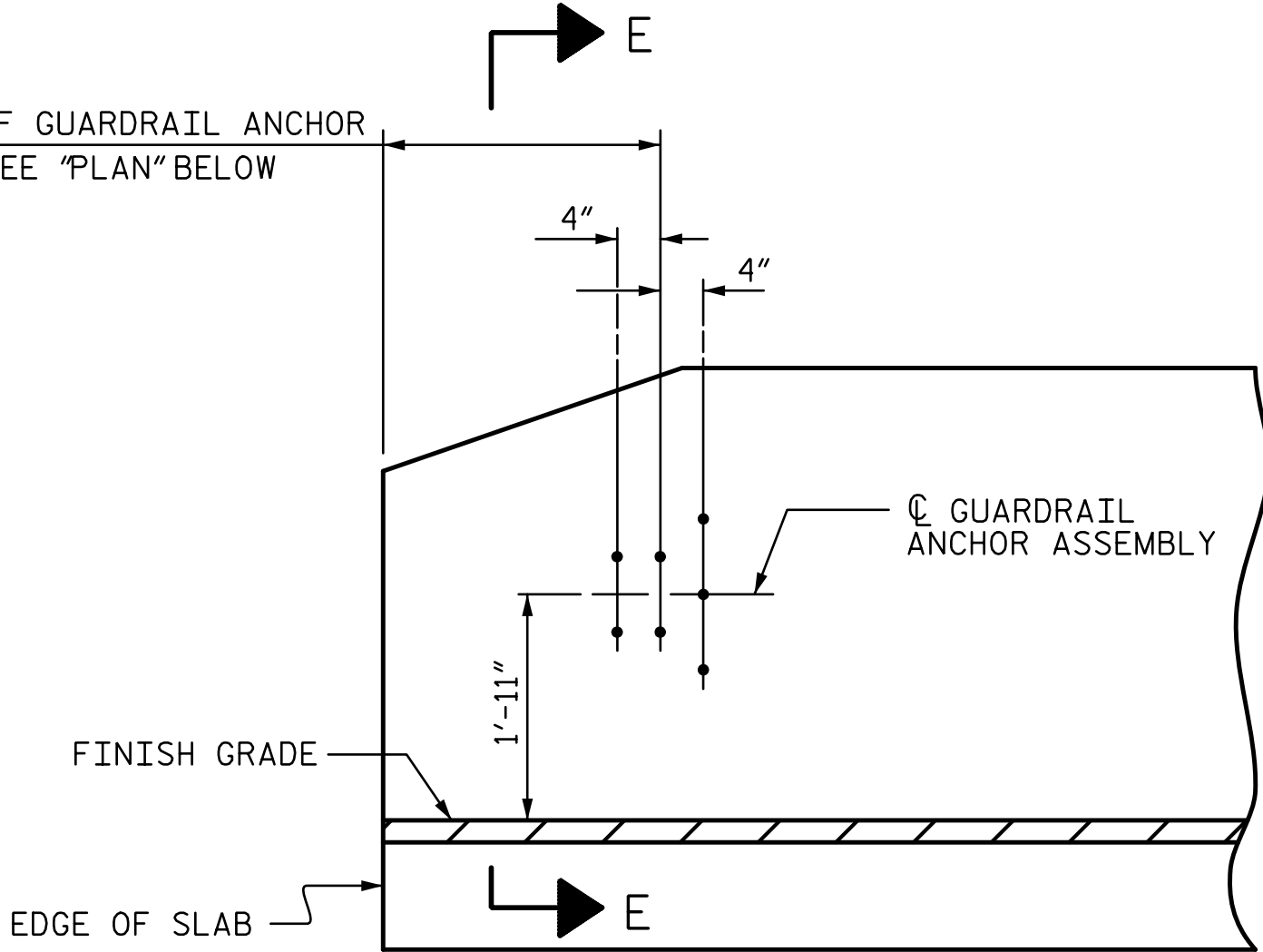
DOCUMENT NOT CONSIDERED
FINAL UNLESS ALL
SIGNATURES COMPLETED

| | |
|--|---------------------|
| DRAWN BY : <u>ATH</u> | DATE : <u>7-16</u> |
| CHECKED BY : <u>JWJ</u> | DATE : <u>2-17</u> |
| DESIGN ENGINEER OF RECORD : <u>JWJ</u> | DATE : <u>5-17</u> |
| DRAWN BY : MAA 5/10 | REV. 12/5/11 MAA/GM |
| CHECKED BY : GM 5/10 | REV. 6/13 MAA/GM |
| | REV. 1/15 MAA/GM |

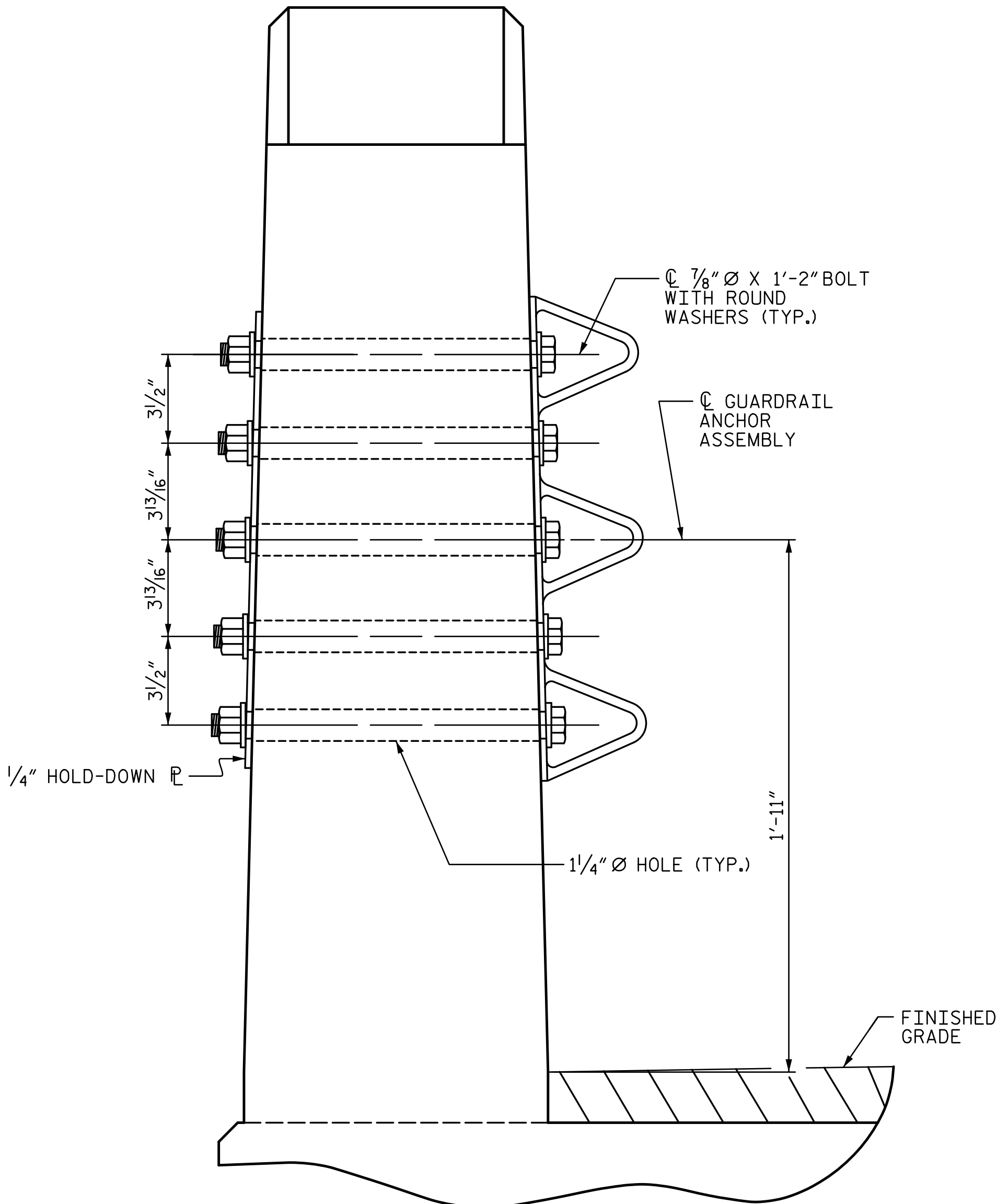


PLAN

FOR LOCATION OF GUARDRAIL ANCHOR ASSEMBLY, SEE "PLAN" BELOW

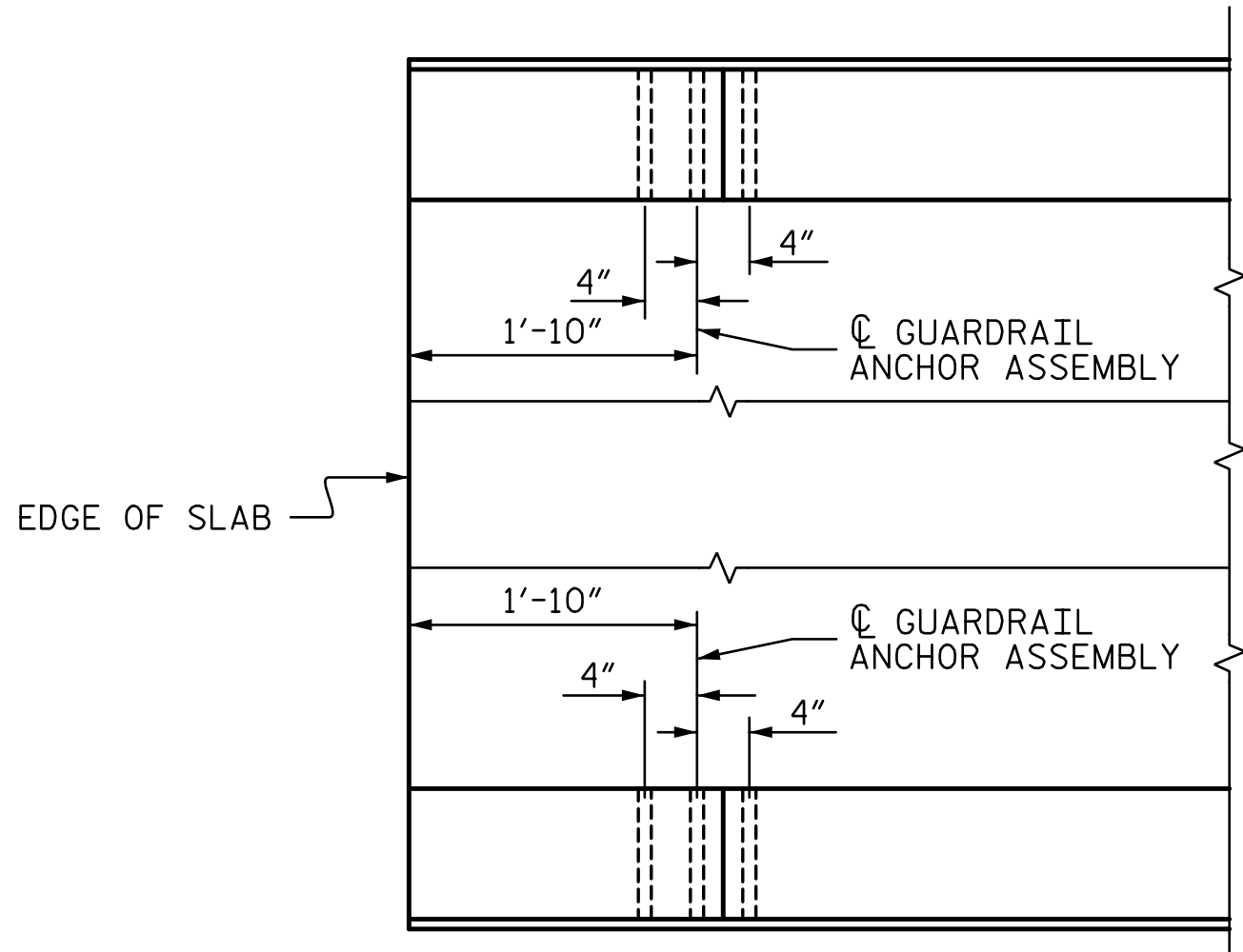


ELEVATION



SECTION E-E

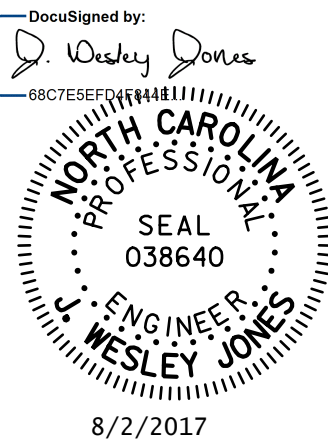
GUARDRAIL ANCHOR ASSEMBLY DETAILS



PLAN

LOCATION OF ANCHORS FOR GUARDRAIL

END BENT #1 SHOWN, END BENT #2 SIMILAR.



STV ENGINEERS, INC.
900 West Trade St., Suite 715
Charlotte, NC 28202
NC License Number F-0991

DOCUMENT NOT CONSIDERED
FINAL UNLESS ALL
SIGNATURES COMPLETED

NOTES

THE GUARDRAIL ANCHOR ASSEMBLY SHALL CONSIST OF A 1/4" HOLD DOWN PLATE AND 7 - 7/8" Ø BOLTS WITH NUTS AND WASHERS.

THE HOLD-DOWN PLATE SHALL CONFORM TO AASHTO M270 GRADE 36. AFTER FABRICATION, THE HOLD-DOWN PLATE SHALL BE HOT-DIP GALVANIZED IN ACCORDANCE WITH AASHTO M111.

BOLTS SHALL CONFORM TO THE REQUIREMENTS OF ASTM A307 AND NUTS SHALL CONFORM TO THE REQUIREMENTS OF AASHTO M291. BOLTS, NUTS AND WASHERS SHALL BE GALVANIZED. (AT THE CONTRACTOR'S OPTION, STAINLESS STEEL BOLTS, NUTS AND WASHERS MAY BE USED AS AN ALTERNATE FOR THE 7/8" Ø GALVANIZED BOLTS, NUTS AND WASHERS, THEY SHALL CONFORM TO OR EXCEED THE MECHANICAL REQUIREMENTS OF ASTM A307. THE USE OF THIS ALTERNATE SHALL BE APPROVED BY THE ENGINEER.)

THE GUARDRAIL ANCHOR ASSEMBLY IS REQUIRED AT ALL POINTS WHERE APPROACH GUARDRAIL IS TO BE ATTACHED TO THE END OF BARRIER RAIL. FOR POINTS OF ATTACHMENT, SEE SKETCH.

AFTER INSTALLATION, THE EXPOSED THREAD OF THE BOLT SHALL BE BURRED WITH A SHARP POINTED TOOL.

THE COST OF THE GUARDRAIL ANCHOR ASSEMBLY SHALL BE INCLUDED IN THE UNIT CONTRACT PRICE BID FOR VERTICAL CONCRETE BARRIER RAIL.

THE VERTICAL REINFORCING BARS MAY BE SHIFTED SLIGHTLY IN THE VERTICAL CONCRETE BARRIER RAIL TO CLEAR ASSEMBLY BOLTS.

THE 1 1/4" Ø HOLES SHALL BE FORMED OR DRILLED WITH A CORE BIT. IMPACT TOOLS WILL NOT BE PERMITTED. ANY CONCRETE DAMAGED BY THIS WORK SHALL BE REPAIRED TO THE SATISFACTION OF THE ENGINEER.



SKETCH SHOWING POINTS OF ATTACHMENTS

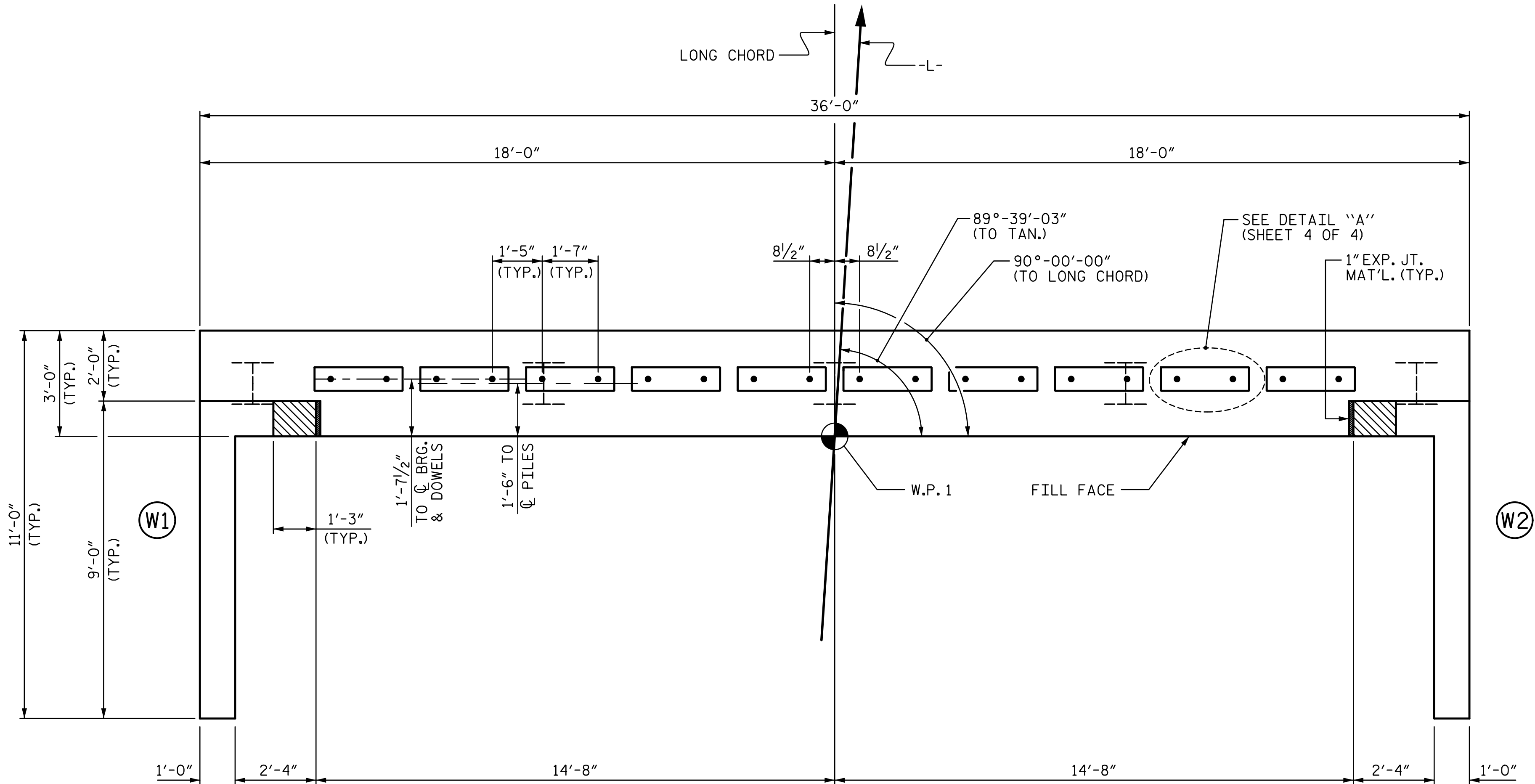
* DENOTES GUARDRAIL ANCHOR ASSEMBLY

PROJECT NO. B-5800
STANLY COUNTY
STATION: 14+88.00 -L-

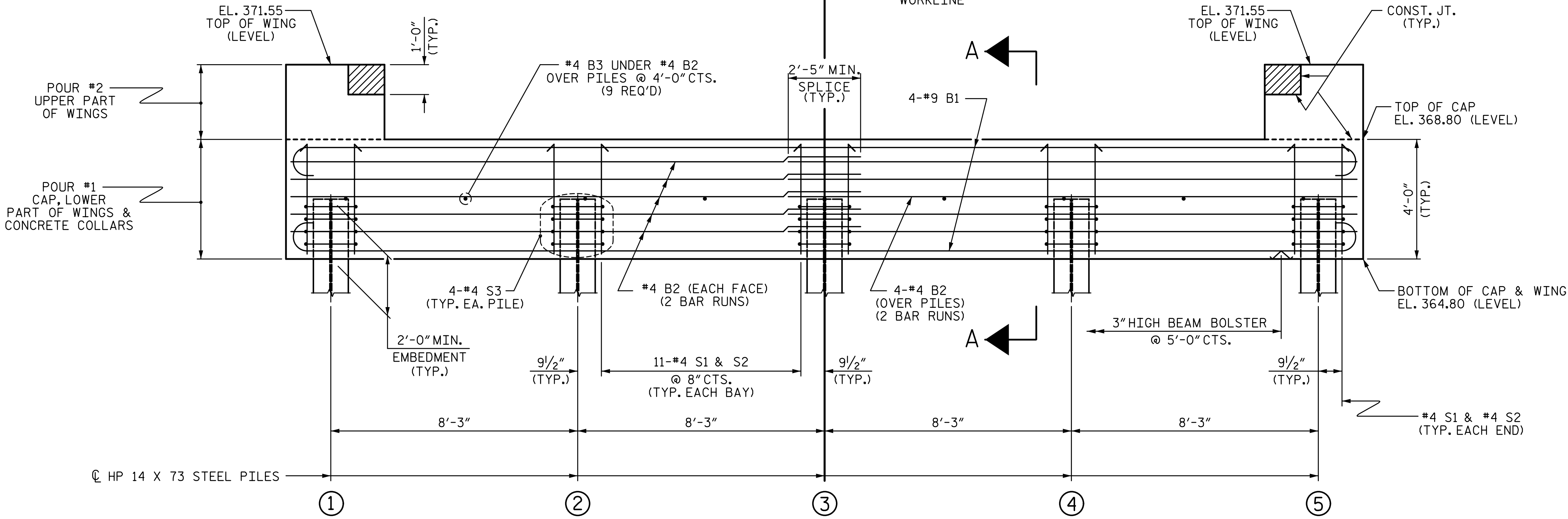
STATE OF NORTH CAROLINA
DEPARTMENT OF TRANSPORTATION
RALEIGH

STANDARD
GUARDRAIL ANCHORAGE
DETAILS FOR VERTICAL
CONCRETE BARRIER RAIL

| REVISIONS | | | | | SHEET NO. |
|-----------|-----|-------|-----|-----|--------------------|
| NO. | BY: | DATE: | NO. | BY: | DATE: |
| 1 | | | 3 | | |
| 2 | | | 4 | | |
| | | | | | TOTAL SHEETS 13 |



PLAN



ELEVATION

WINGS NOT SHOWN FOR CLARITY.
FOR SECTION A-A, SEE SHEET 4 OF 4.
CONCRETE COLLARS FOR STEEL PILES NOT SHOWN IN PLAN AND ELEVATION VIEWS FOR CLARITY.
SEE "CORROSION PROTECTION FOR STEEL PILES DETAIL", SHEET 4 OF 4.

| | | | |
|-----------------------------|------|---------|------|
| DRAWN BY : | LEM | DATE : | 3-17 |
| CHECKED BY : | JWJ | DATE : | 3-17 |
| DESIGN ENGINEER OF RECORD : | JWJ | DATE : | 5-17 |
| DRAWN BY : | WJH | 12/11 | |
| CHECKED BY : | AAC | 12/11 | |
| REV. | 4/15 | MAA/TMG | |



STV ENGINEERS, INC.
900 West Trade St., Suite 715
Charlotte, NC 28202
NC License Number F-0991

DOCUMENT NOT CONSIDERED
FINAL UNLESS ALL
SIGNATURES COMPLETED

NOTES

STIRRUPS IN CAP MAY BE SHIFTED AS NECESSARY TO CLEAR DOWELS.

THE CONCRETE IN THE SHADED AREA OF THE WING SHALL BE POURED AFTER THE VERTICAL CONCRETE BARRIER RAIL IS CAST IF SLIP FORMING IS USED.

FOR PILE SPLICE DETAILS, SEE SHEET 4 OF 4.

FOR WING DETAILS, SEE SHEET 3 OF 4.

PROJECT NO. B-5800

STANLEY COUNTY

STATION: 14+88.00 -L-

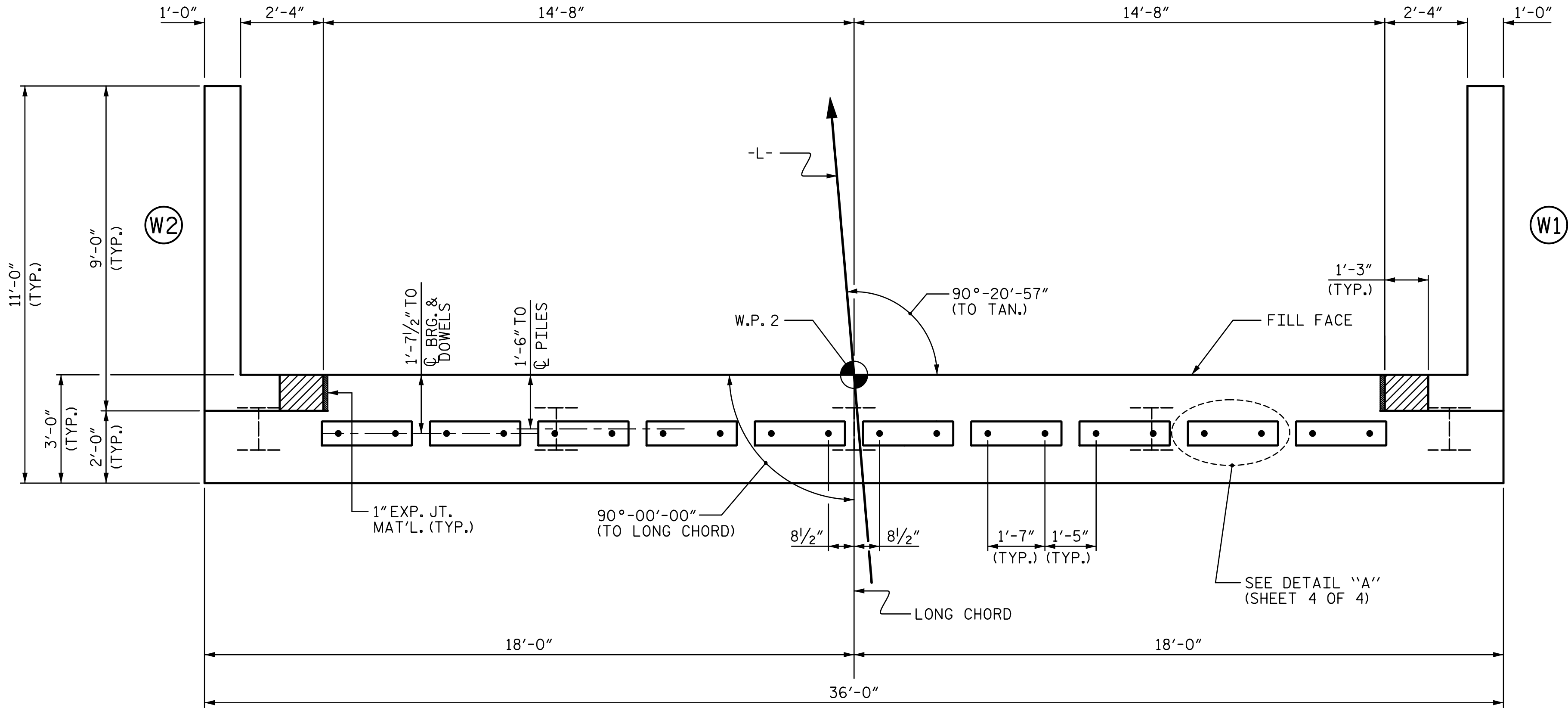
SHEET 1 OF 4

STATE OF NORTH CAROLINA
DEPARTMENT OF TRANSPORTATION
RALEIGH

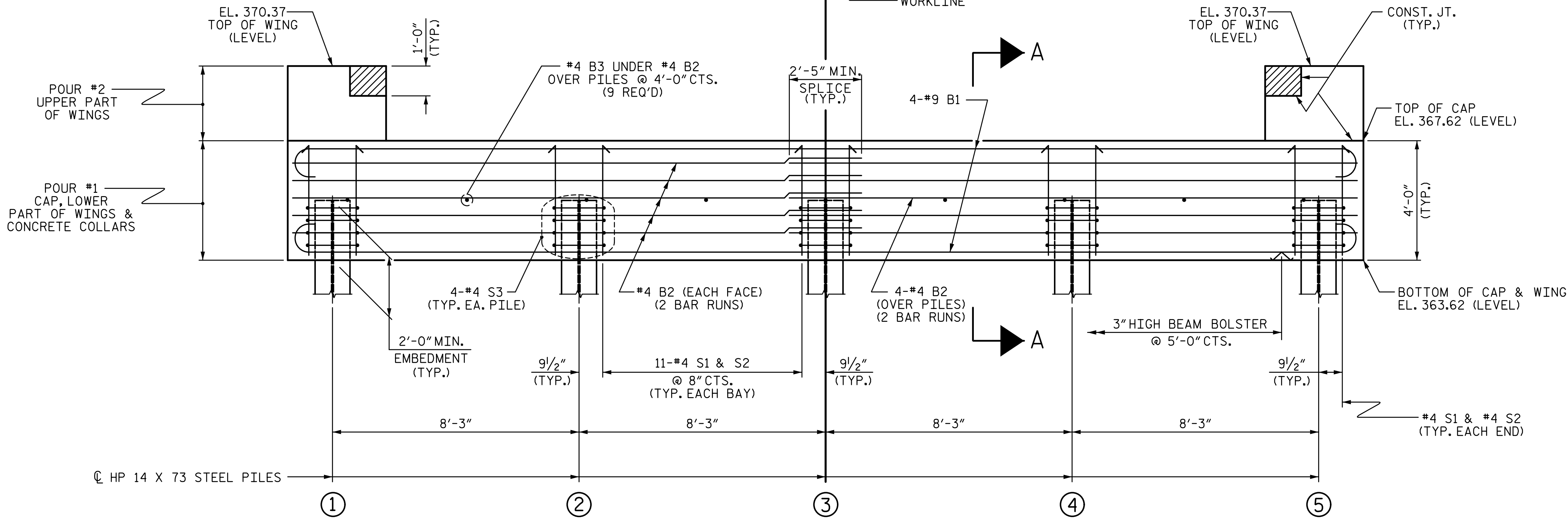
SUBSTRUCTURE
END BENT No. 1

| REVISIONS | | | | SHEET NO. | |
|-----------|-----|-------|-----|--------------|-------|
| NO. | BY: | DATE: | NO. | BY: | DATE: |
| 1 | | | 3 | | |
| 2 | | | 4 | | |
| | | | | S-8 | |
| | | | | TOTAL SHEETS | 13 |

STD. NO. EB_30_90S4



PLAN



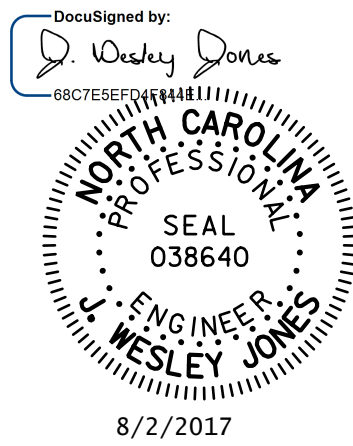
ELEVATION

| | |
|---------------------------------|-------------------|
| DRAWN BY : LEM | DATE : 3-17 |
| CHECKED BY : JWJ | DATE : 3-17 |
| DESIGN ENGINEER OF RECORD : JWJ | DATE : 5-17 |
| DRAWN BY : WJH 12/11 | REV. 4/15 MAA/TMG |
| CHECKED BY : AAC 12/11 | |

WINGS NOT SHOWN FOR CLARITY.
FOR SECTION A-A, SEE SHEET 4 OF 4.
CONCRETE COLLARS FOR STEEL PILES NOT SHOWN IN PLAN AND ELEVATION VIEWS FOR CLARITY.
SEE "CORROSION PROTECTION FOR STEEL PILES DETAIL", SHEET 4 OF 4.



STV ENGINEERS, INC.
900 West Trade St., Suite 715
Charlotte, NC 28202
NC License Number F-0991



DOCUMENT NOT CONSIDERED
FINAL UNLESS ALL
SIGNATURES COMPLETED

NOTES

STIRRUPS IN CAP MAY BE SHIFTED AS NECESSARY TO CLEAR DOWELS.

THE CONCRETE IN THE SHADED AREA OF THE WING SHALL BE POURED AFTER THE VERTICAL CONCRETE BARRIER RAIL IS CAST IF SLIP FORMING IS USED.

FOR PILE SPLICE DETAILS, SEE SHEET 4 OF 4.

FOR WING DETAILS, SEE SHEET 3 OF 4.

PROJECT NO. B-5800

STANLEY COUNTY

STATION: 14+88.00 -L-

SHEET 2 OF 4

STATE OF NORTH CAROLINA
DEPARTMENT OF TRANSPORTATION
RALEIGH

SUBSTRUCTURE

END BENT No. 2

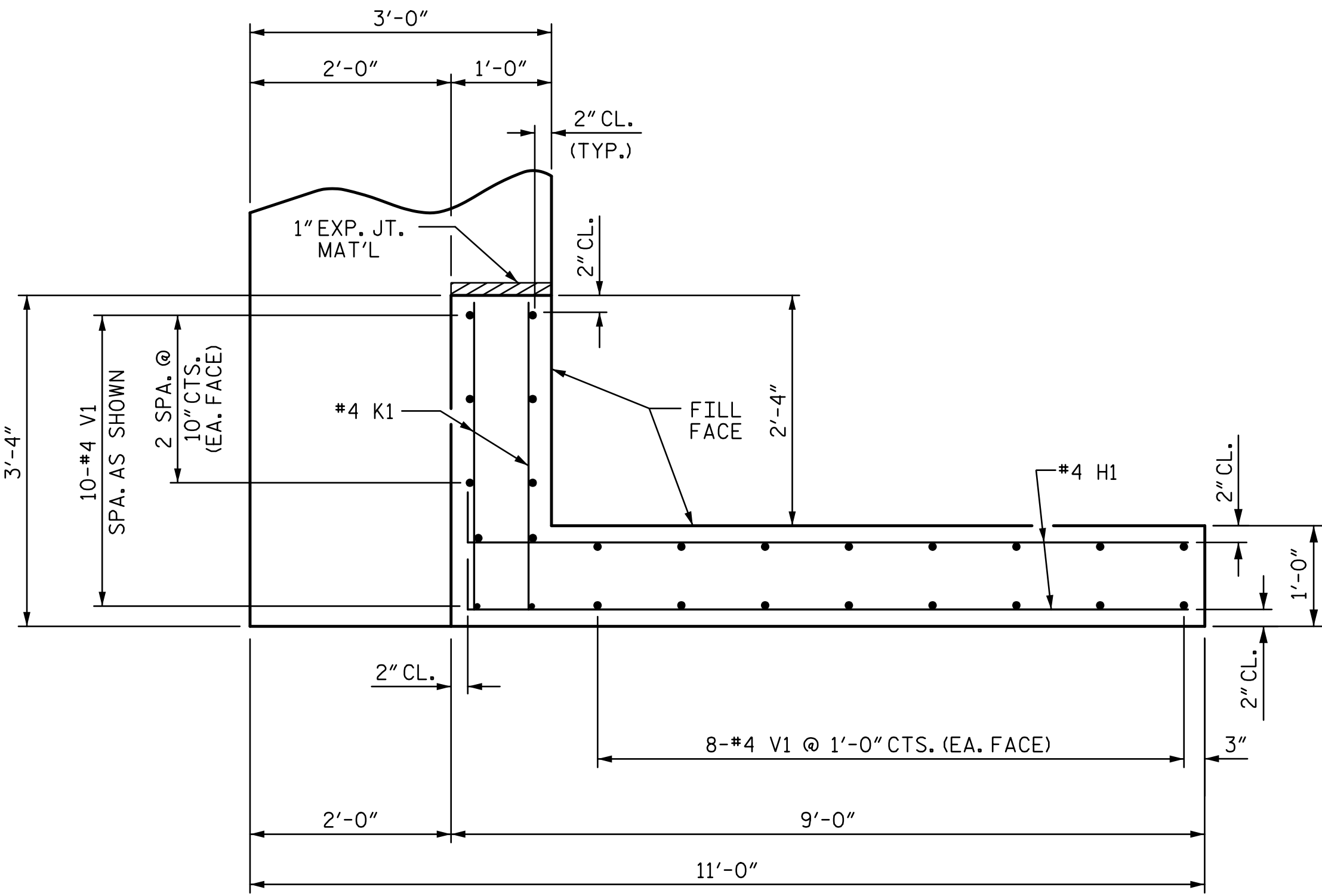
| REVISIONS | | | | | SHEET NO. S-9 |
|-----------|-----|-------|-----|-----|------------------|
| NO. | BY: | DATE: | NO. | BY: | |
| 1 | | | 3 | | TOTAL SHEETS 13 |
| 2 | | | 4 | | |

R:\Structures\ustation\401.019.B-5800_SMU.EB03_010.830160.dgn

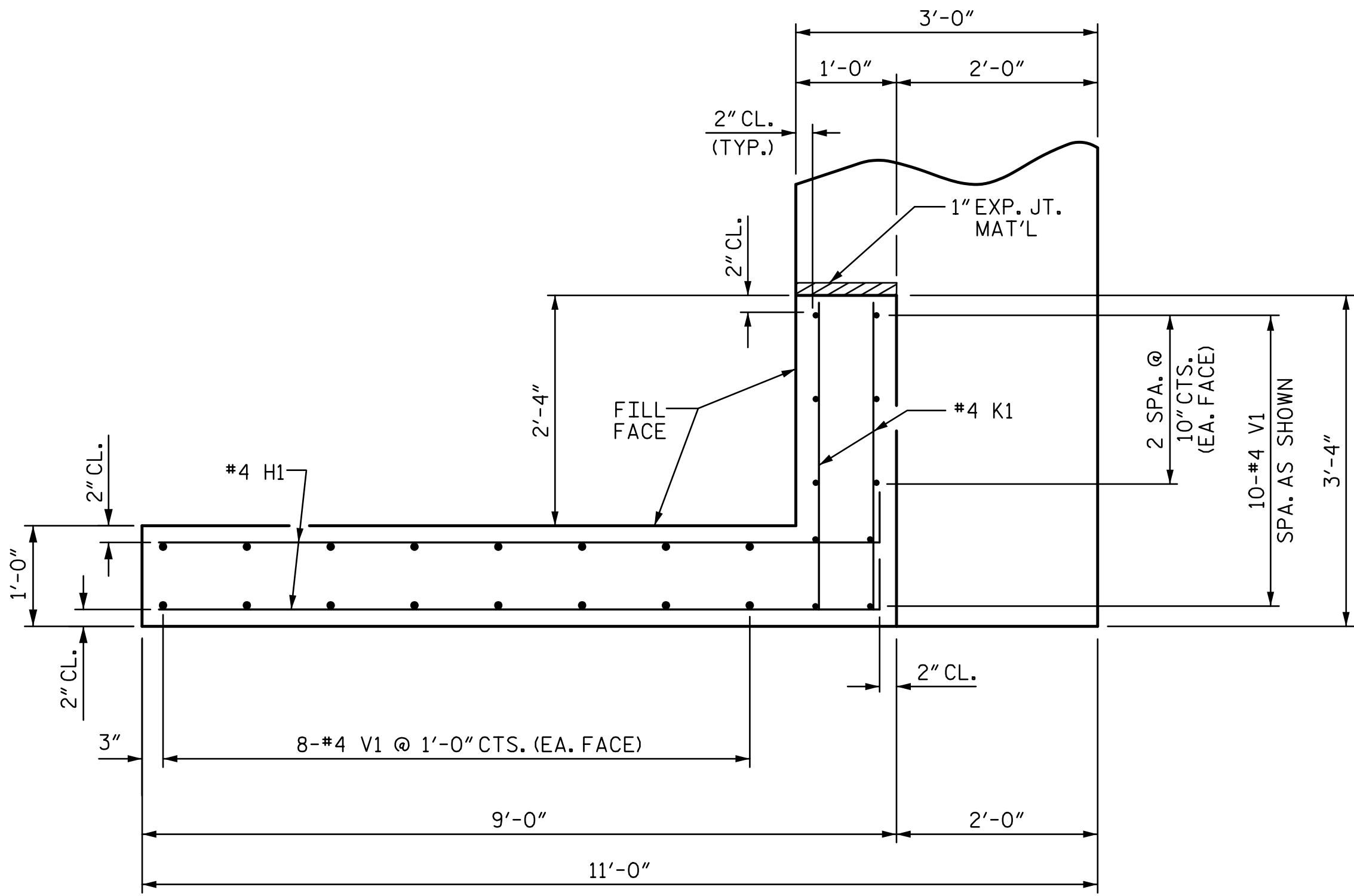
12:33:51 PM

8/2/2017

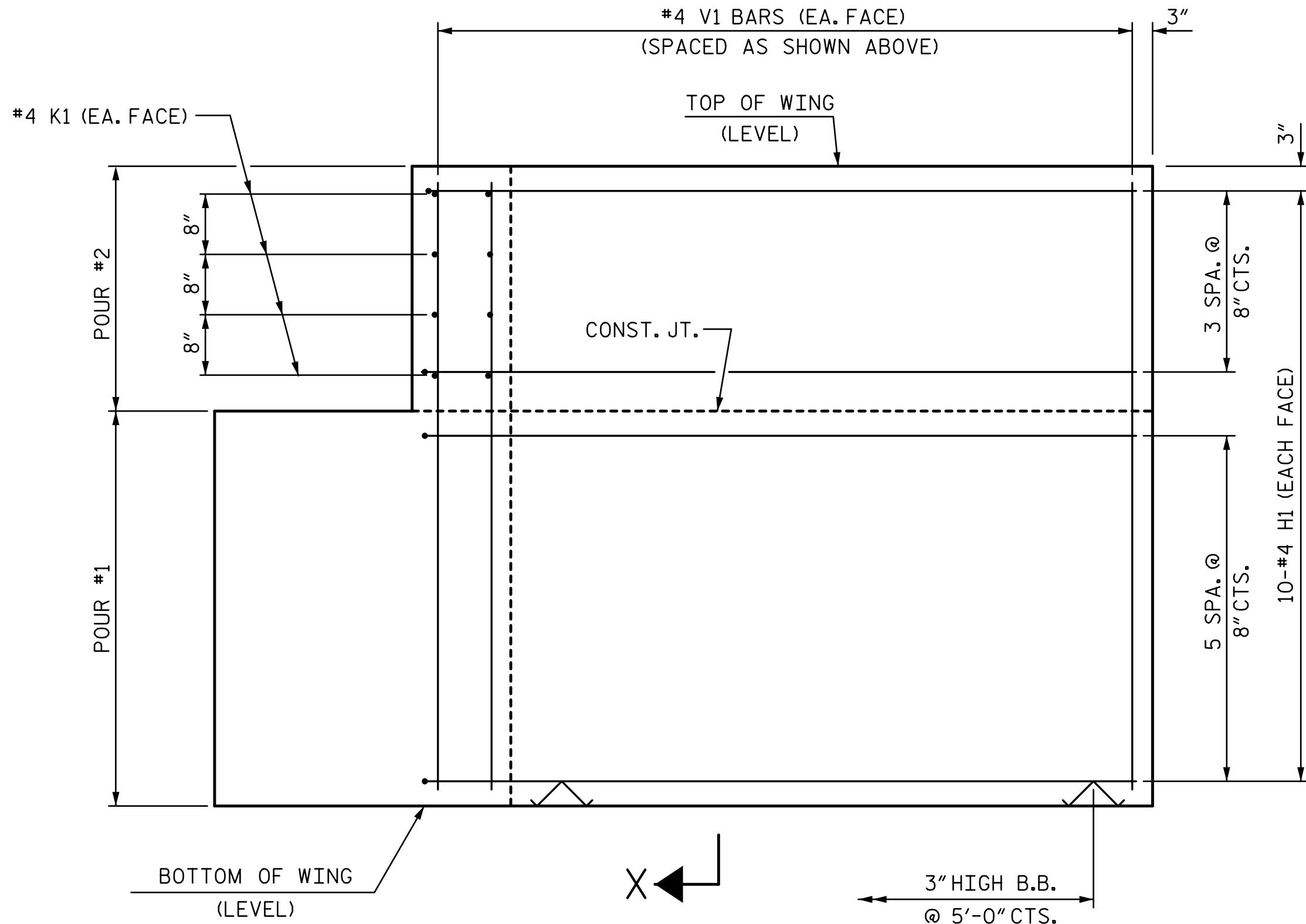
Jones



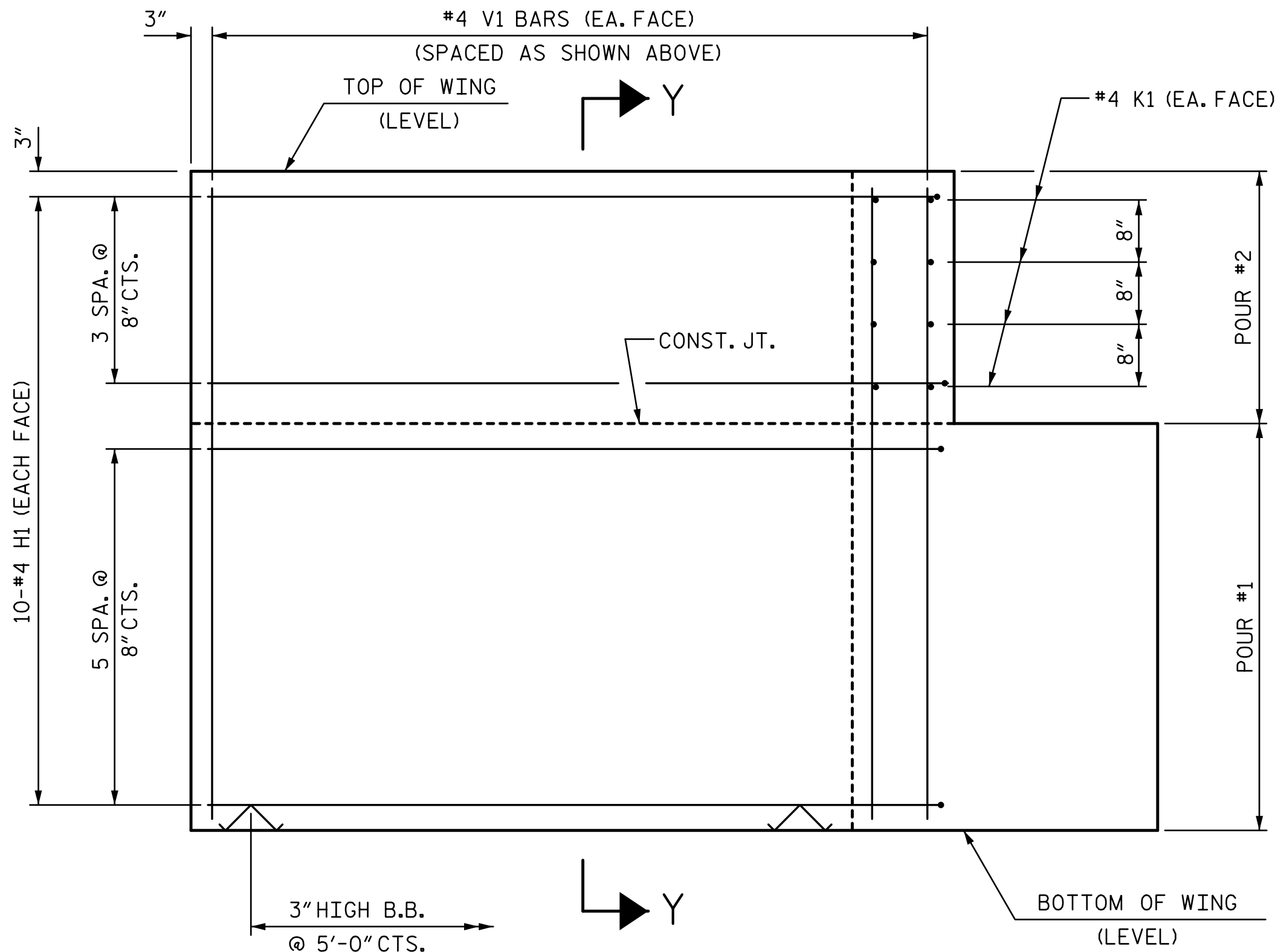
PLAN OF WING (W1)



PLAN OF WING (W2)



ELEVATION OF WING (W1)



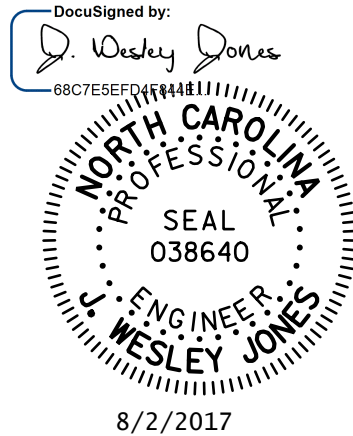
ELEVATION OF WING (W2)

WING DETAILS

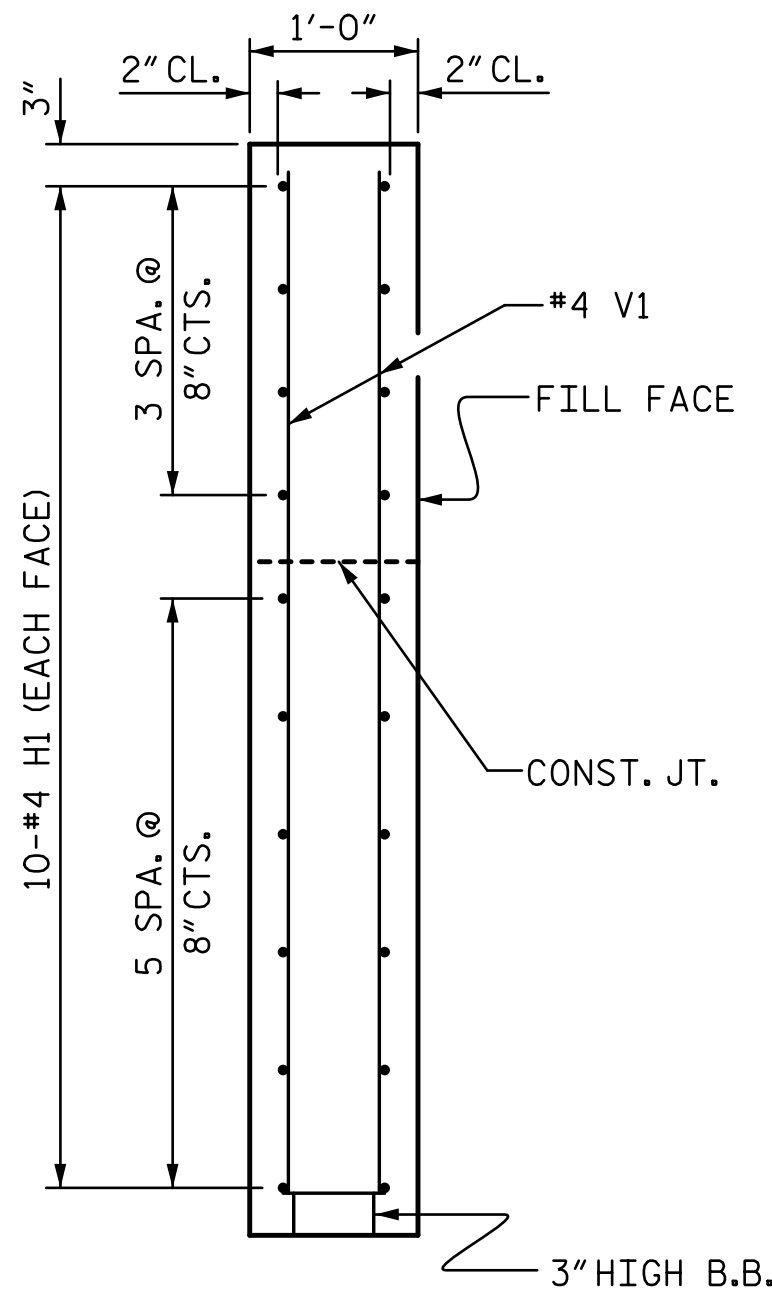
| | | | |
|-----------------------------|-----|--------|-----------|
| DRAWN BY : | LEM | DATE : | 3-17 |
| CHECKED BY : | JWJ | DATE : | 3-17 |
| DESIGN ENGINEER OF RECORD : | JWJ | DATE : | 5-17 |
| DRAWN BY : | WJH | 12/11 | REV. 4/15 |
| CHECKED BY : | AAC | 12/11 | MAA/TMG |



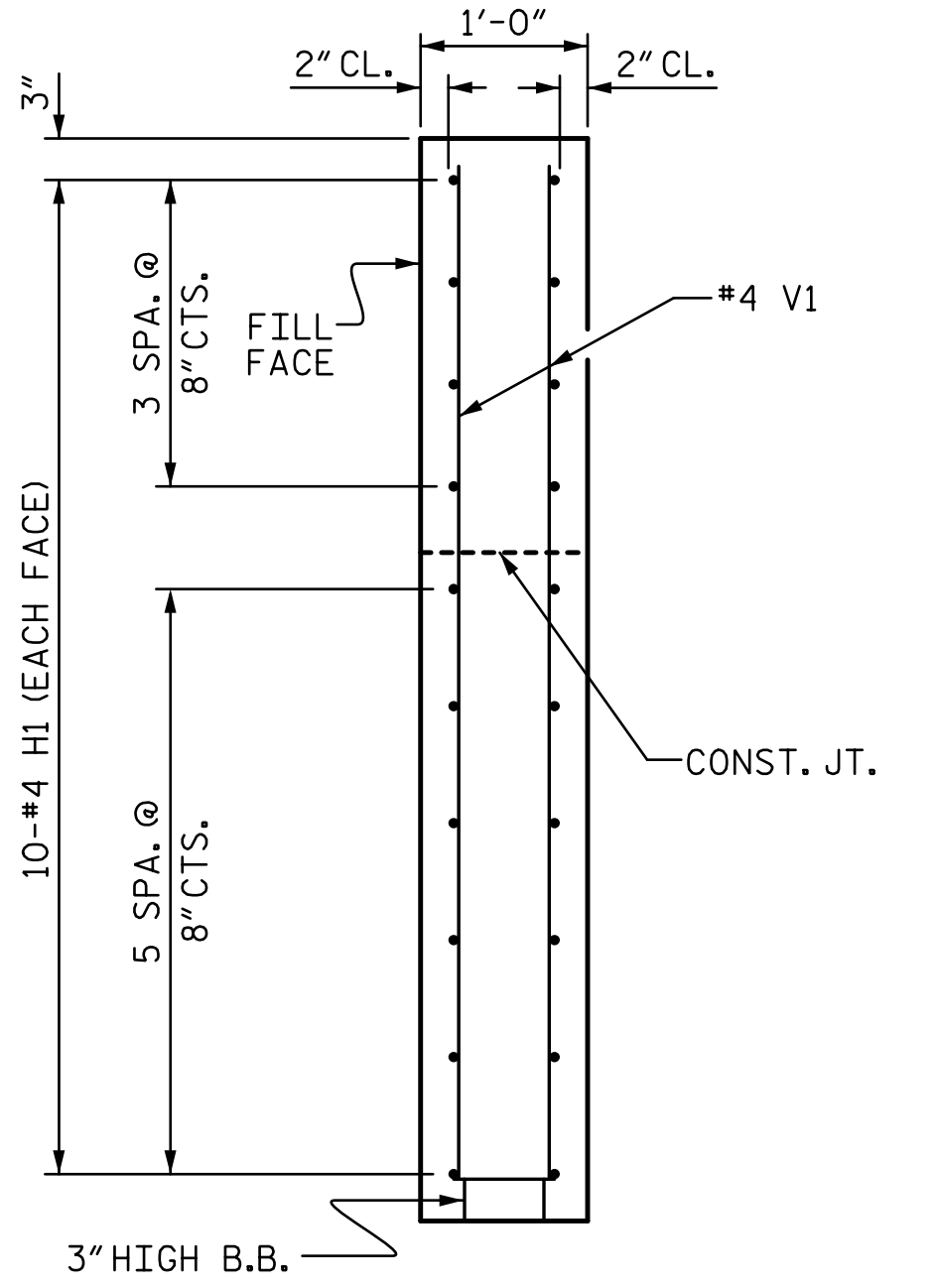
STV ENGINEERS, INC.
900 West Trade St., Suite 715
Charlotte, NC 28202
NC License Number F-0991



DOCUMENT NOT CONSIDERED
FINAL UNLESS ALL
SIGNATURES COMPLETED



SECTION X-X



SECTION Y-Y

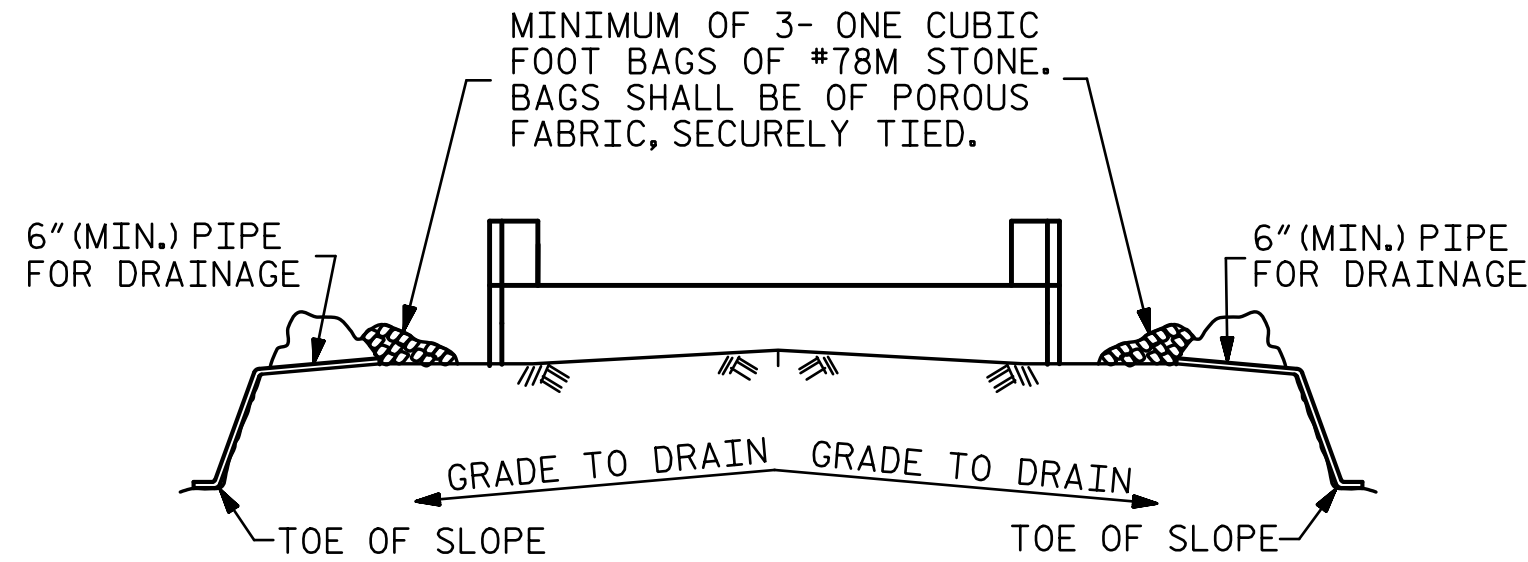
PROJECT NO. **B-5800**
STANLY COUNTY
STATION: **14+88.00 -L-**

SHEET 3 OF 4

| REVISIONS | | | | | SHEET NO. |
|-----------------|-----|-------|-----|-----|-----------|
| NO. | BY: | DATE: | NO. | BY: | |
| 1 | | | 3 | | S-10 |
| 2 | | | 4 | | |
| TOTAL SHEETS 13 | | | | | |

STD. NO. EB_30_90S4

R:\Structures\ustation\401.021.B-5800_SMU.EB04_OIL.830160.dgn 8/2/2017 12:33:53 PM Jones

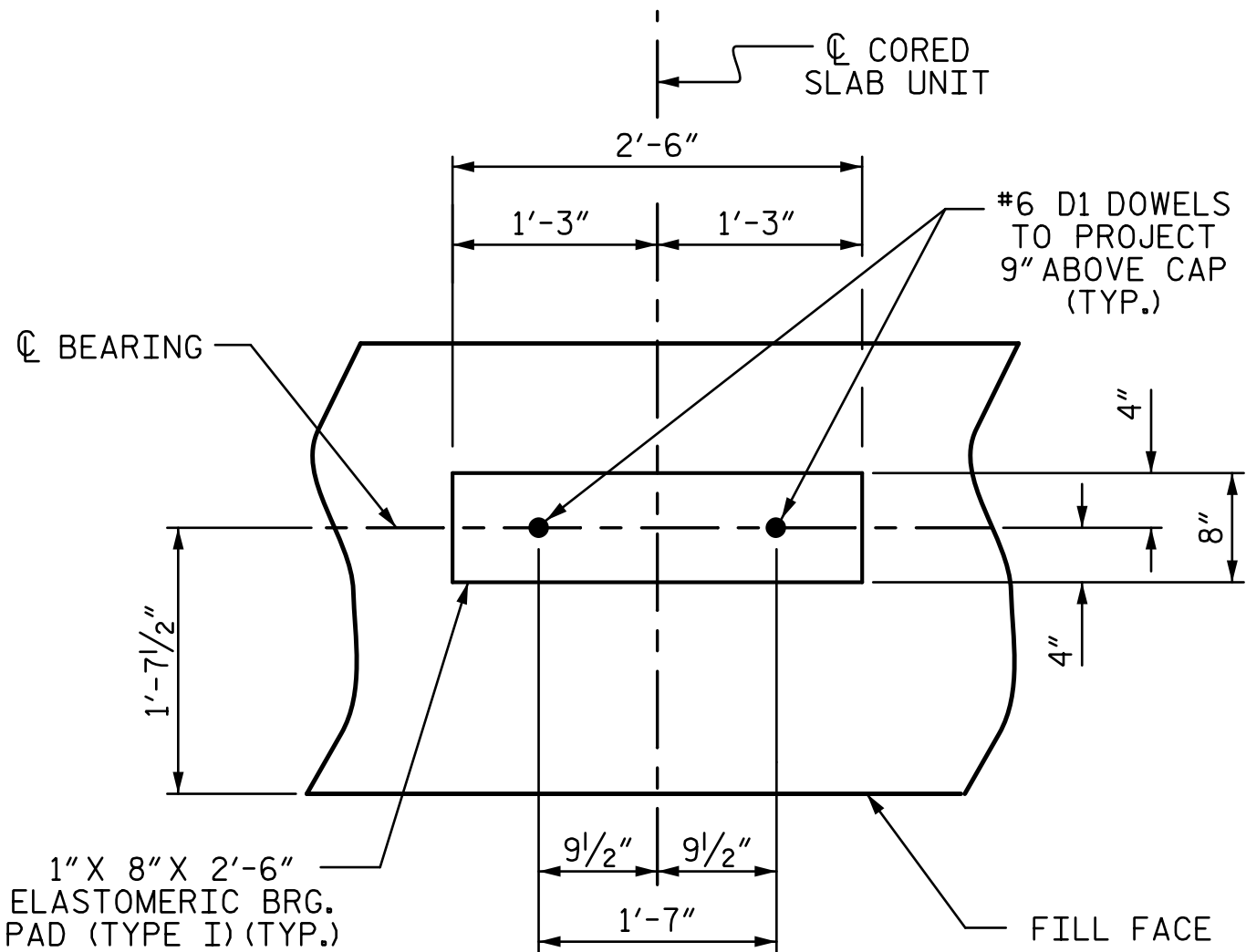


BAGGED STONE AND PIPE SHALL BE PLACED IMMEDIATELY AFTER COMPLETION OF END BENT EXCAVATION. PIPE MAY BE EITHER CONCRETE, CORRUGATED STEEL, CORRUGATED ALUMINUM ALLOY, OR CORRUGATED PLASTIC. PERFORATED PIPE WILL NOT BE ALLOWED.

BAGGED STONE SHALL REMAIN IN PLACE UNTIL THE ENGINEER DIRECTS THAT IT BE REMOVED. THE CONTRACTOR SHALL REMOVE AND DISPOSE OF SILT ACCUMULATIONS AT BAGGED STONE WHEN SO DIRECTED BY THE ENGINEER. BAGS SHALL BE REMOVED AND REPLACED WHENEVER THE ENGINEER DETERMINES THAT THEY HAVE DETERIORATED AND LOST THEIR EFFECTIVENESS.

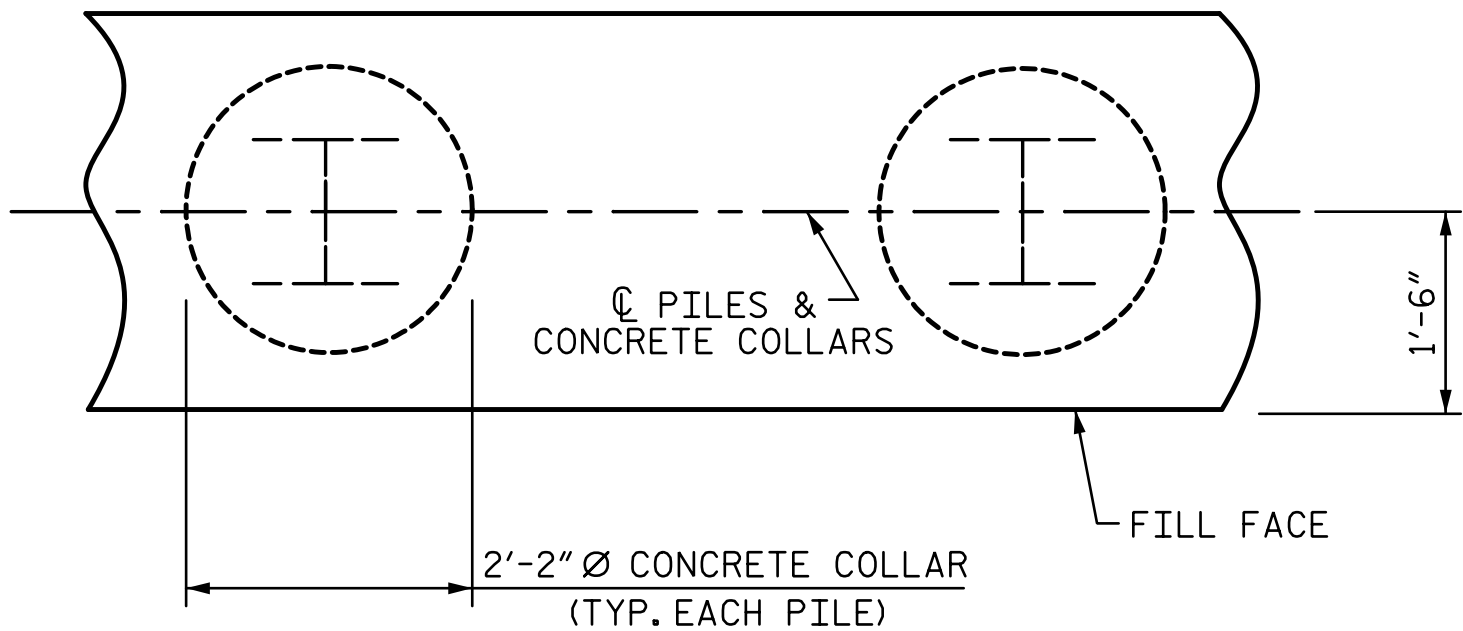
NO SEPARATE PAYMENT WILL BE MADE FOR THIS WORK AND THE ENTIRE COST OF THIS WORK SHALL BE INCLUDED IN THE UNIT CONTRACT PRICE BID FOR THE SEVERAL PAY ITEMS.

TEMPORARY DRAINAGE AT END BENT



DETAIL "A"

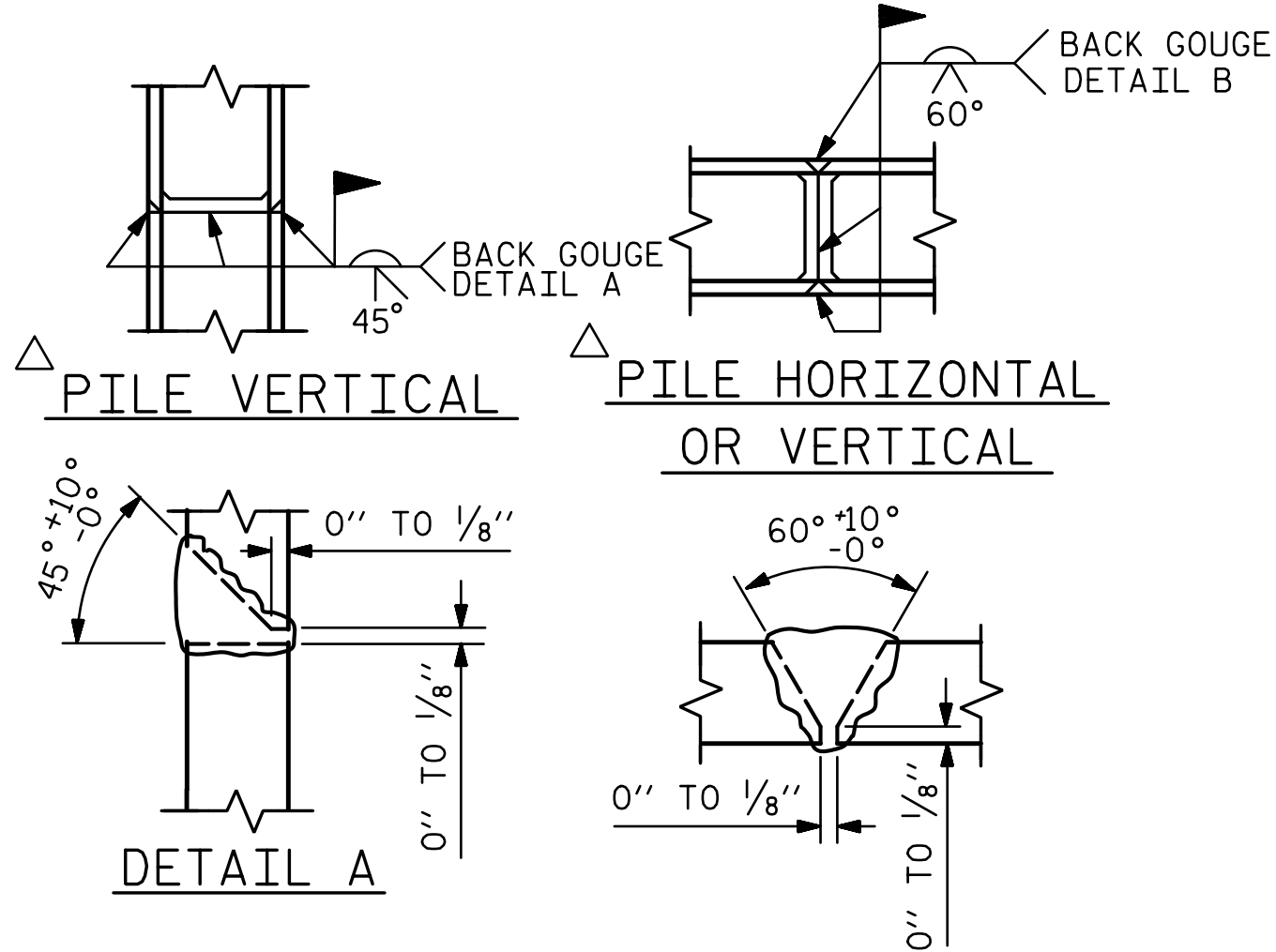
(END BENT No.1 SHOWN, END BENT No.2 SIMILAR BY ROTATION)



PLAN

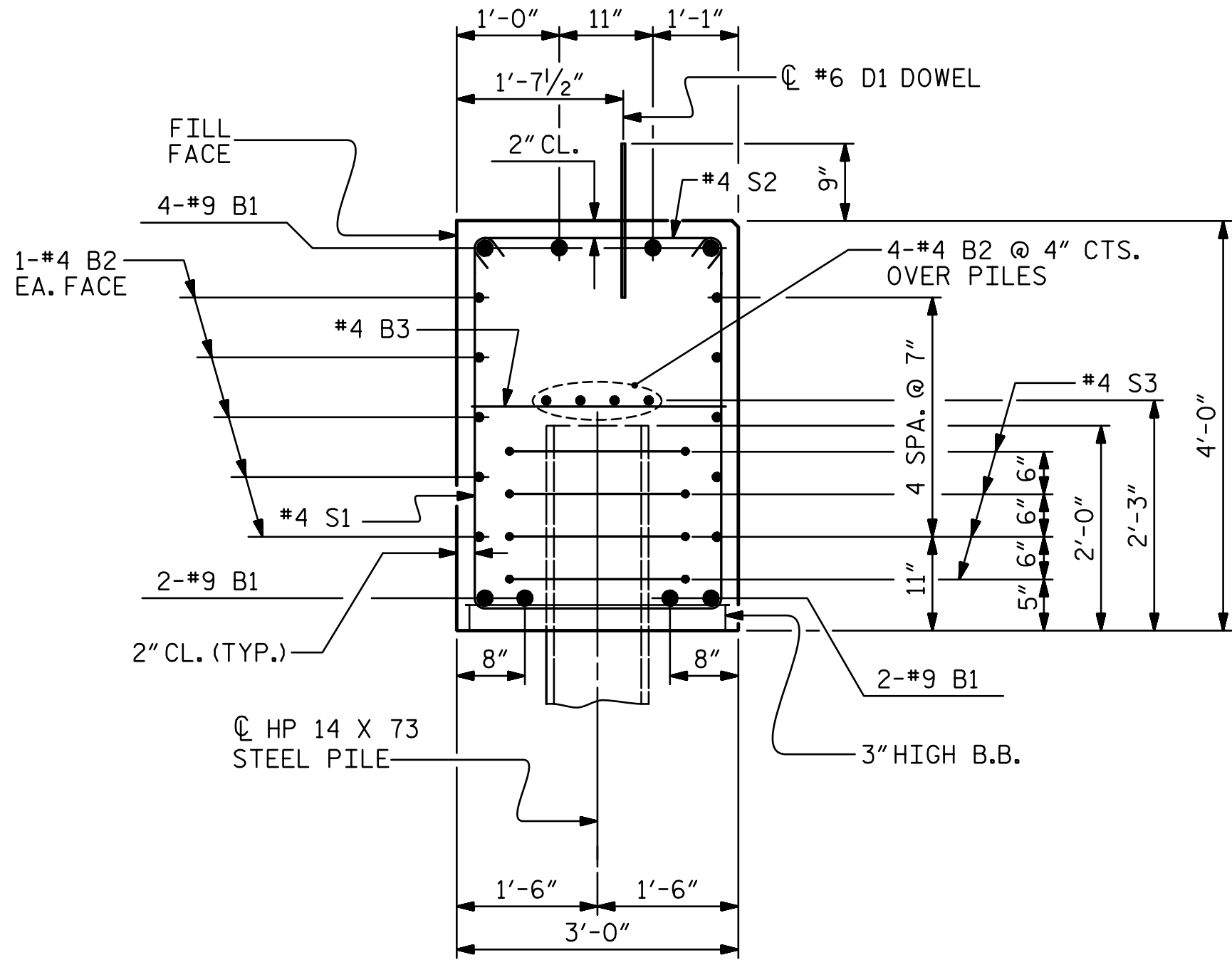
CORROSION PROTECTION FOR STEEL PILES DETAIL

(END BENT No. 1 SHOWN, END BENT No. 2 SIMILAR BY ROTATION)



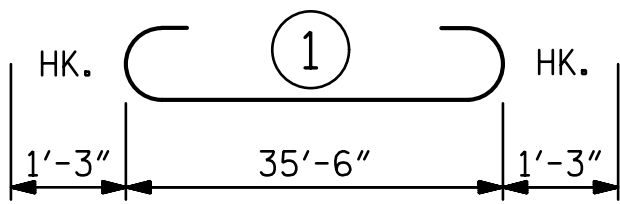
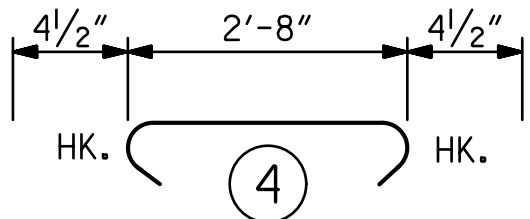
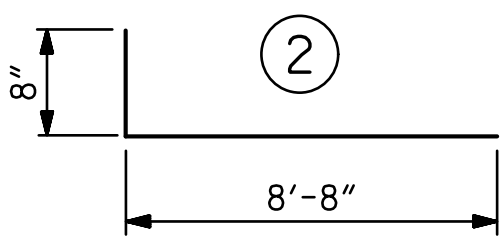
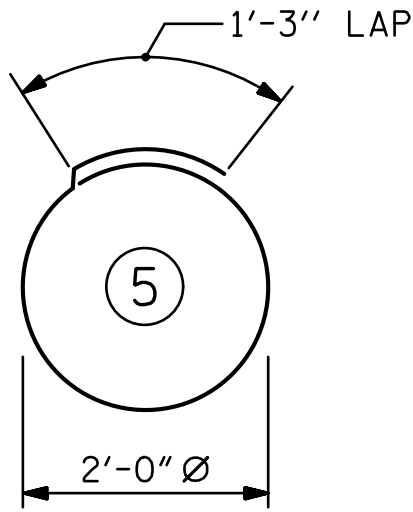
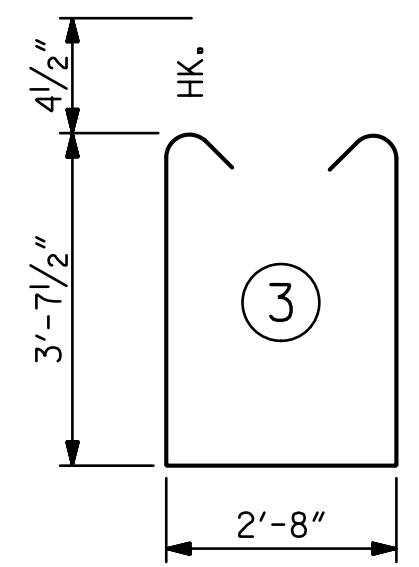
PILE SPLICE DETAILS

POSITION OF PILE DURING WELDING.



SECTION A-A

(CONCRETE COLLAR NOT SHOWN FOR CLARITY, SEE "CORROSION PROTECTION FOR STEEL PILES DETAIL.")

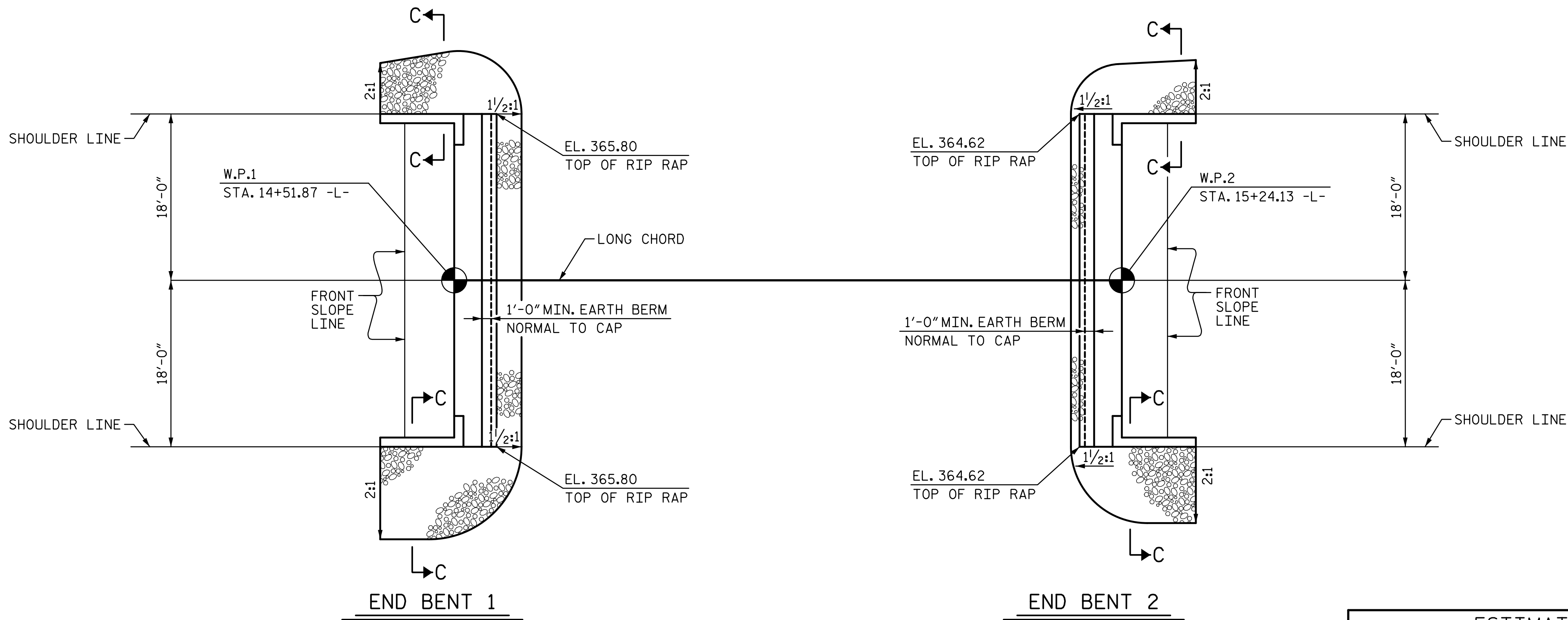
| BAR TYPES | | | BILL OF MATERIAL FOR ONE END BENT | | | | | |
|---|--|--|---|--|--|--|--|--|
|  | | |  | | | | | |
|  | | |  | | | | | |
|  | | | | | | | | |
| ALL BAR DIMENSIONS ARE OUT TO OUT. | | | | | | | | |
| END BENT No. 1 HP 14 X 73 STEEL PILES NO: 5 LIN. FT.= 75 | | | END BENT No. 2 HP 14 X 73 STEEL PILES NO: 5 LIN. FT.= 75 | | | | | |
| STEEL PILE PTS. EA. = 5 | | | STEEL PILE PTS. EA. = 5 | | | | | |
| | | | REINFORCING STEEL (FOR ONE END BENT) | | | | | |

R:\Structures\ustation\401.023_B-5800_SMJ_RR_012_830160.dgn

12:33:55 PM

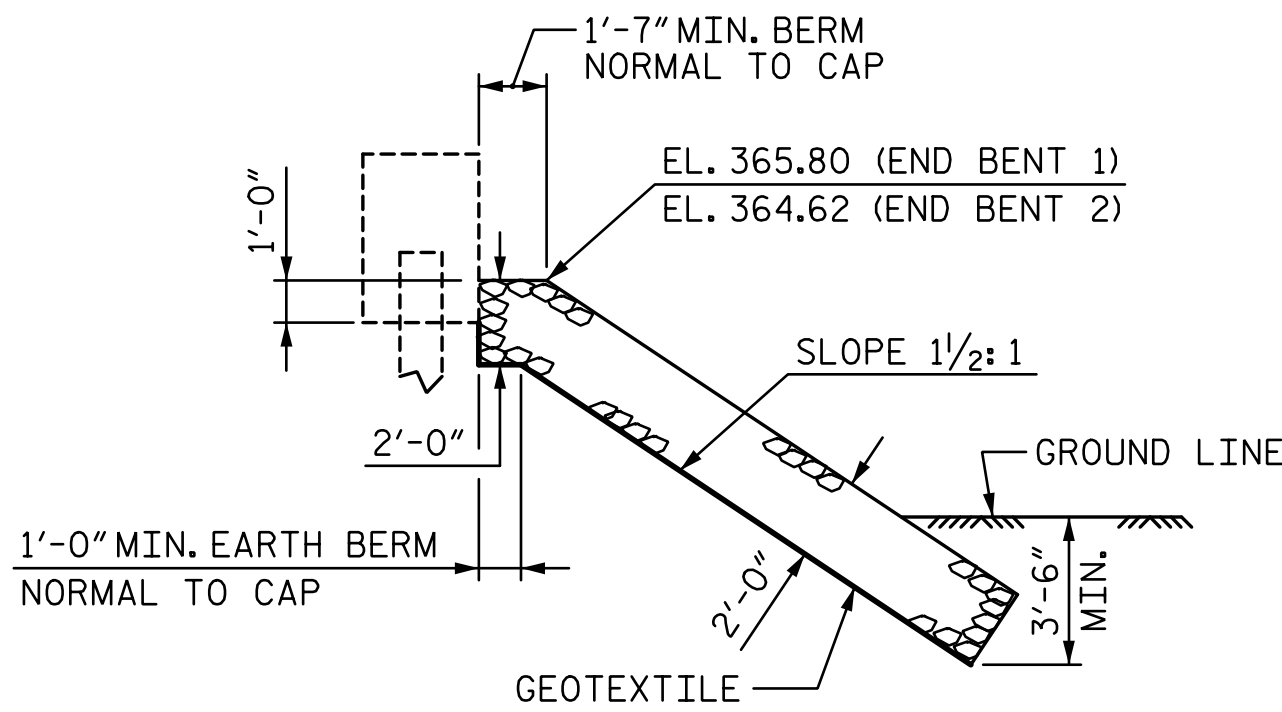
8/2/2017

Jones

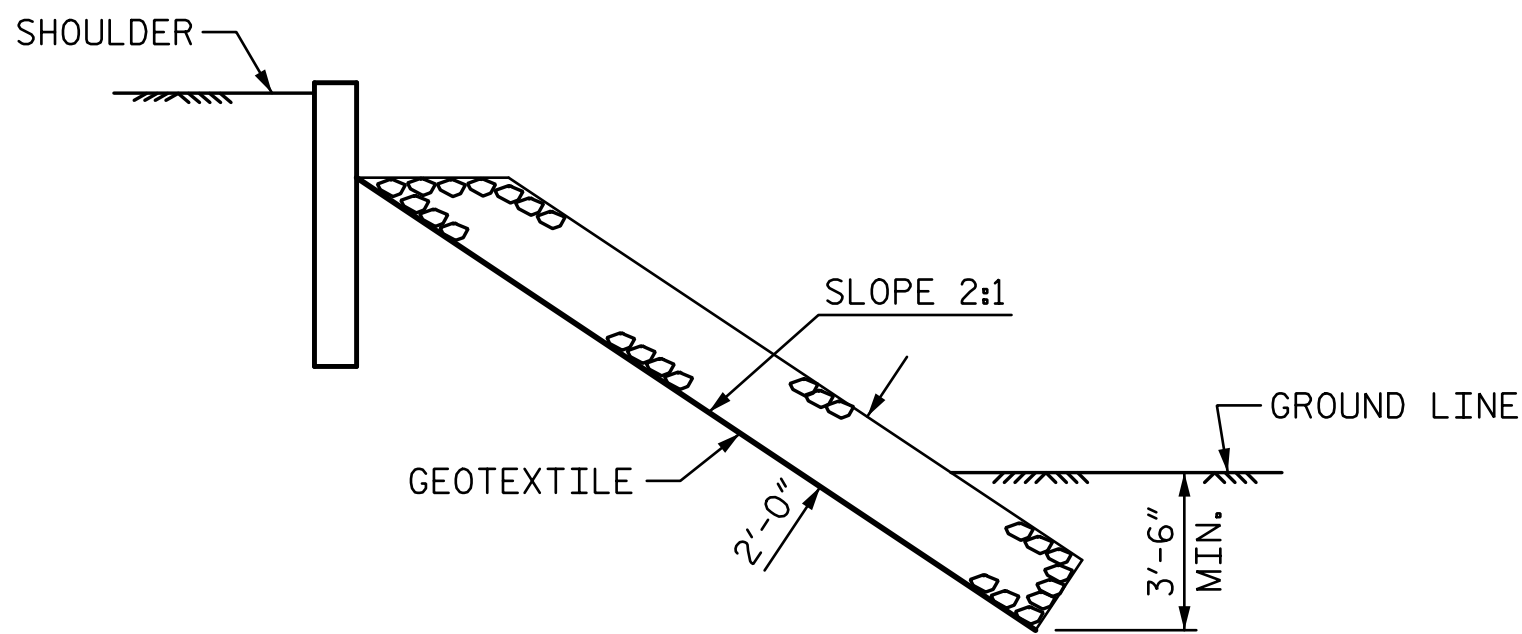


PLAN

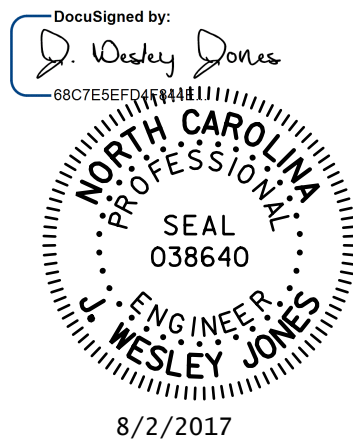
| ESTIMATED QUANTITIES | | |
|-------------------------------|--------------------------------------|----------------------------|
| BRIDGE @ STA. 14+88.00 -L- | RIP RAP CLASS II (2'-0" THICK) | GEOTEXTILE FOR DRAINAGE |
| | TONS | SQUARE YARDS |
| END BENT 1 | 70 | 75 |
| END BENT 2 | 55 | 60 |



LONG CHORD SECTION
BERM RIP RAPPED



SECTION C-C



STV 100 YEARS
STV ENGINEERS, INC.
900 West Trade St., Suite 715
Charlotte, NC 28202
NC License Number F-0991

DOCUMENT NOT CONSIDERED
FINAL UNLESS ALL
SIGNATURES COMPLETED

PROJECT NO. B-5800
STANLY COUNTY
STATION: 14+88.00 -L-

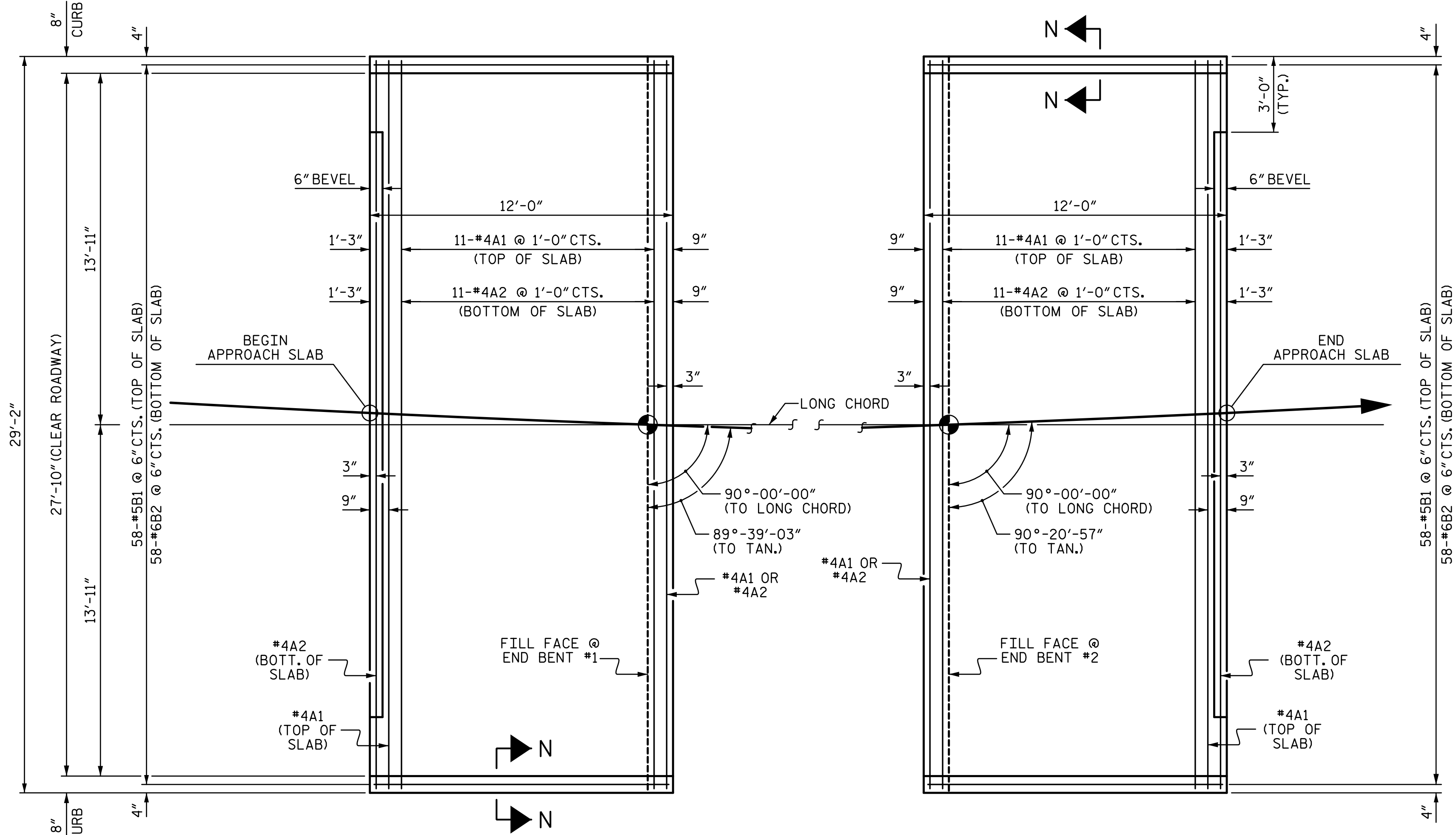
STATE OF NORTH CAROLINA
DEPARTMENT OF TRANSPORTATION
RALEIGH

RIP RAP DETAILS

DRAWN BY : ATH DATE : 7-16
CHECKED BY : JWJ DATE : 5-17
DESIGN ENGINEER OF RECORD : JWJ DATE : 5-17

| REVISIONS | | | | | SHEET NO. S-12 |
|-----------|-----|-------|-----|-----|-------------------|
| NO. | BY: | DATE: | NO. | BY: | |
| 1 | | | 3 | | TOTAL SHEETS |
| 2 | | | 4 | | 13 |

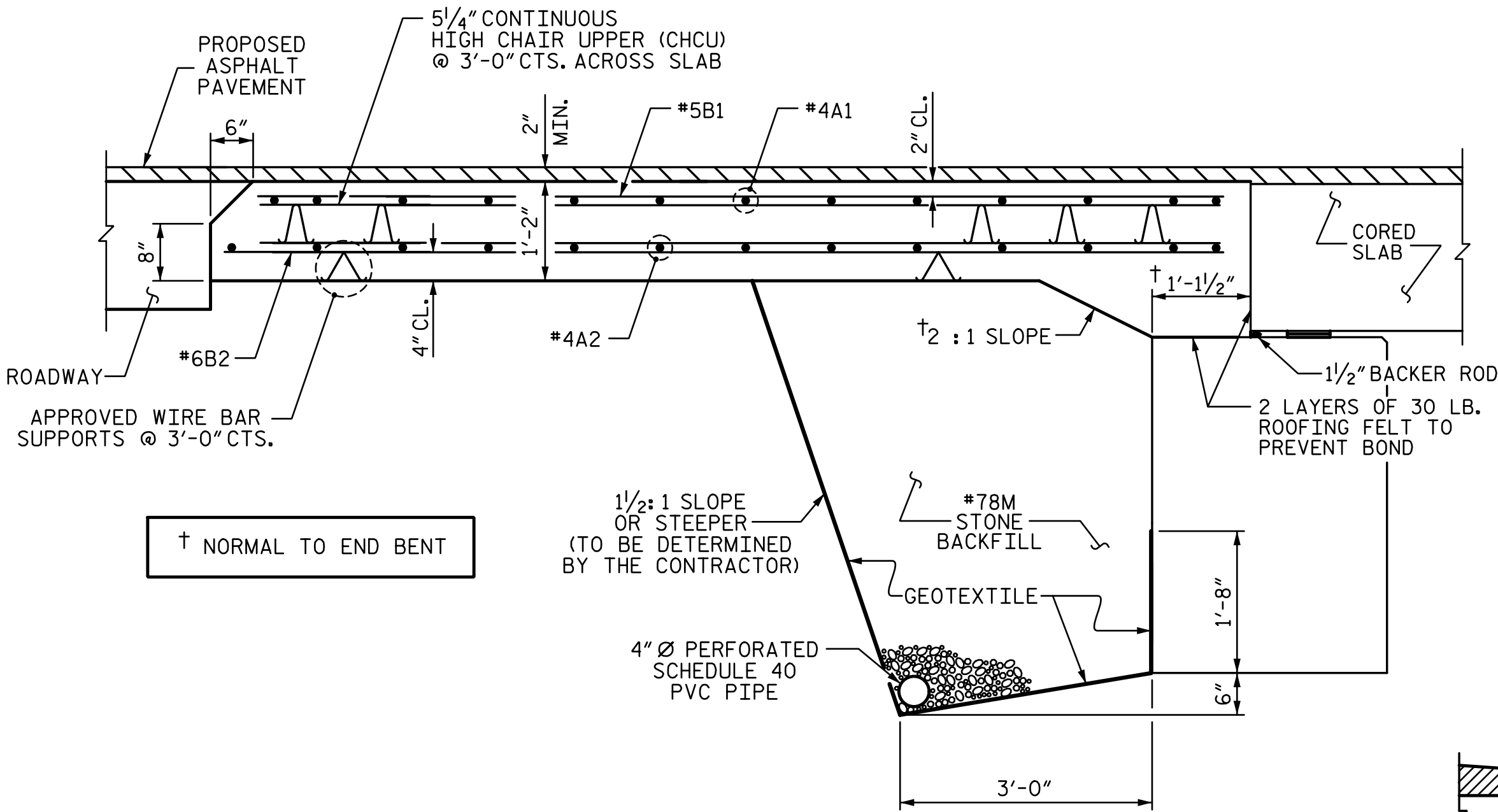
R:\Structures\ustation\401.025_B-5800_SMJ.BAS.013.830160.dgn 8/2/2017 12:33:57 PM Jones



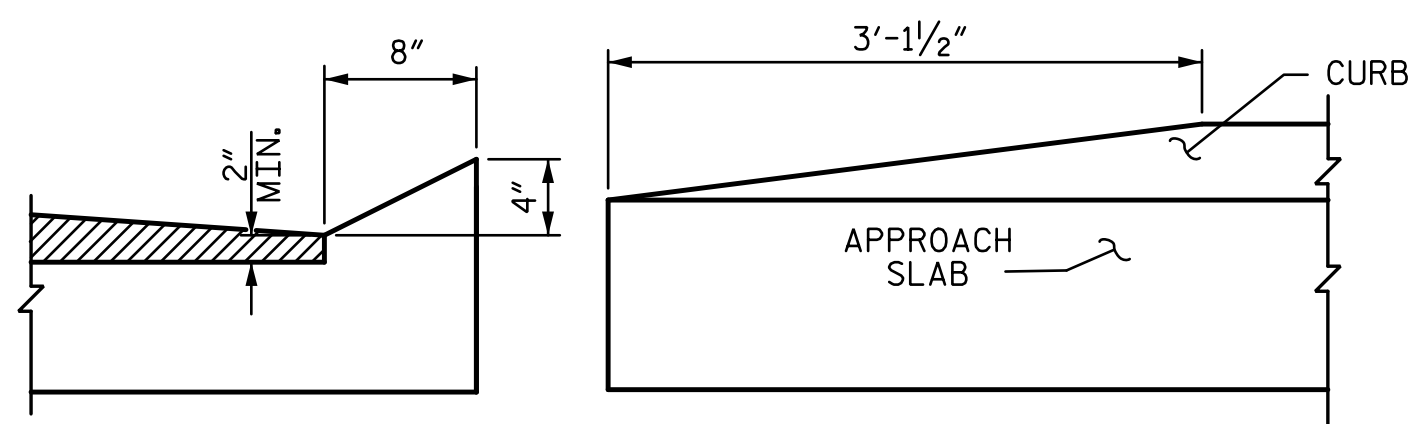
PLAN @ END BENT #1

PLAN @ END BENT #2

DIMENSIONS SHOWN ARE TYPICAL FOR BOTH APPROACH SLABS



SECTION THRU SLAB



SECTION N-N

END OF CURB WITHOUT SHOULDER BERM GUTTER

CURB DETAILS

NOTES

FOR BRIDGE APPROACH FILL INCLUDING GEOTEXTILE, 4"Ø DRAINAGE PIPE, AND #78M STONE BACKFILL, SEE ROADWAY PLANS.

GEOTEXTILE SHALL BE TYPE 1 IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS SECTION 1056.

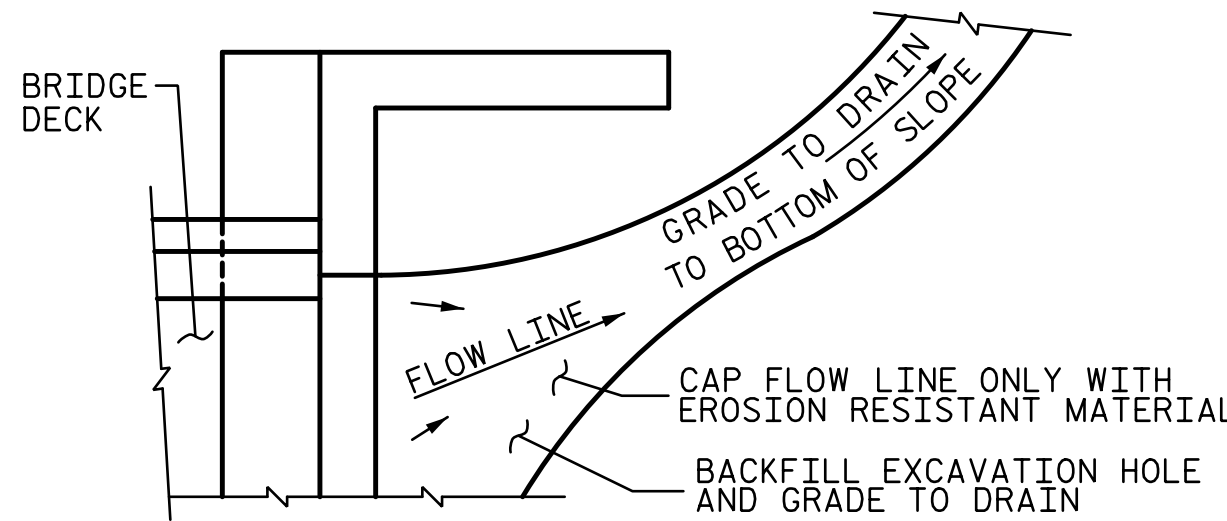
#78M STONE BACKFILL (CLASS V SELECT MATERIAL) SHALL BE IN ACCORDANCE WITH STANDARD SPECIFICATIONS SECTION 1016.

#78M STONE BACKFILL IS TO BE CONTINUOUS ALONG FILL FACE OF BACKWALL FROM OUTSIDE EDGE TO OUTSIDE EDGE OF APPROACH SLAB.

FOR THE 4"Ø DRAINAGE PIPE OUTLET(S), SEE ROADWAY STANDARD DRAWINGS.

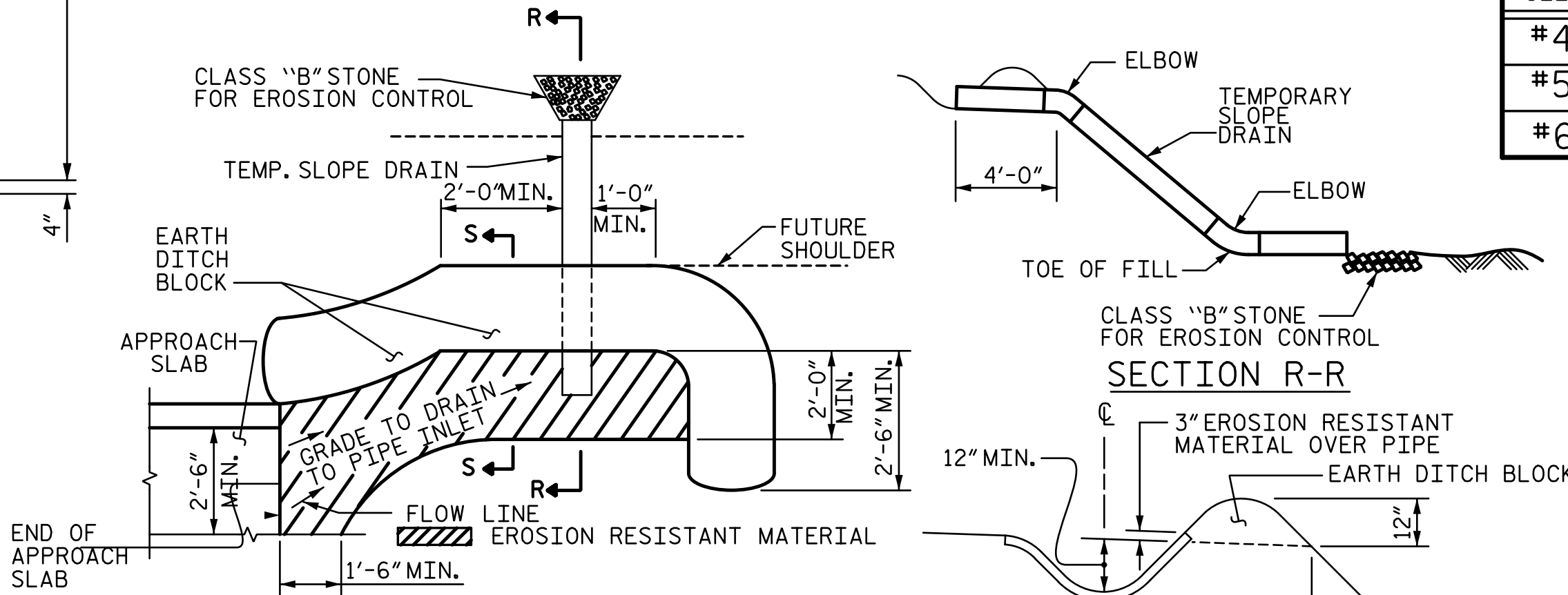
AREA BETWEEN THE WINGWALL AND APPROACH SLAB SHALL BE GRADED TO DRAIN THE WATER AWAY FROM THE FILL FACE OF THE BRIDGE AND SHALL BE PAVED. SEE ROADWAY PLANS.

APPROACH SLAB GROOVING IS NOT REQUIRED.



NOTE: IF THE APPROACH SLAB IS NOT CONSTRUCTED IMMEDIATELY AFTER THE BACKFILLING OF THE END BENT EXCAVATION, GRADE TO DRAIN TO THE BOTTOM OF THE SLOPE AND PROVIDE EROSION RESISTANT MATERIAL, SUCH AS FIBERGLASS ROVING OR AS DIRECTED BY THE ENGINEER TO PREVENT SOIL EROSION AND TO PROTECT THE AREA ADJACENT TO THE STRUCTURE, THE CONTRACTOR WILL BE REQUIRED TO REMOVE THESE MATERIALS PRIOR TO CONSTRUCTION OF THE APPROACH SLAB.

TEMPORARY DRAINAGE DETAIL



NOTE: IMMEDIATELY AFTER THE CONSTRUCTION OF THE APPROACH SLAB, THE CONTRACTOR SHALL PROVIDE TEMPORARY BERM AND SLOPE DRAIN. CONTRACTOR SHALL GRADE TO PIPE INLET AND PROVIDE EROSION RESISTANT MATERIAL AS SHOWN. THE EROSION RESISTANT MATERIAL SHALL BE EITHER 1) ASPHALT PLANT MIX, TYPE 1 OR TYPE 2, MIN. 2" DEPTH, 2) EROSION CONTROL MAT, OR 3) CONCRETE, AS DIRECTED BY THE ENGINEER. THE SLOPE DRAIN SHALL CONSIST OF A NON-PERFORATED TEMPORARY DRAINAGE PIPE, 12 INCHES IN DIAMETER.

PLAN VIEW

TEMPORARY BERM AND SLOPE DRAIN DETAILS

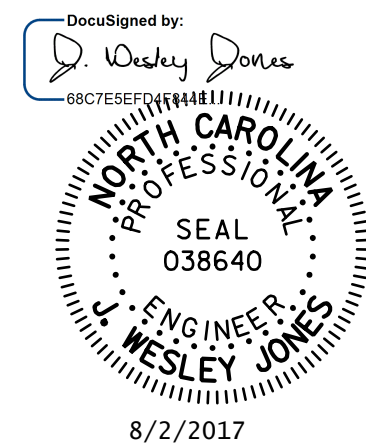
(TO BE USED WHEN SHOULDER BERM GUTTER IS REQUIRED)

| BILL OF MATERIAL | | | | | | |
|----------------------------------|-----|------|------|---------|--------|--|
| APPROACH SLAB AT EB #1 | | | | | | |
| BAR | NO. | SIZE | TYPE | LENGTH | WEIGHT | |
| * A1 | 13 | #4 | STR | 28'-10" | 250 | |
| A2 | 13 | #4 | STR | 28'-10" | 250 | |
| * B1 | 58 | #5 | STR | 11'-2" | 676 | |
| B2 | 58 | #6 | STR | 11'-8" | 1016 | |
| REINFORCING STEEL | | | | LBS. | 1266 | |
| * EPOXY COATED REINFORCING STEEL | | | | LBS. | 926 | |
| CLASS AA CONCRETE | | | | C. Y. | 17.7 | |
| APPROACH SLAB AT EB #2 | | | | | | |
| BAR | NO. | SIZE | TYPE | LENGTH | WEIGHT | |
| * A1 | 13 | #4 | STR | 28'-10" | 250 | |
| A2 | 13 | #4 | STR | 28'-10" | 250 | |
| * B1 | 58 | #5 | STR | 11'-2" | 676 | |
| B2 | 58 | #6 | STR | 11'-8" | 1016 | |
| REINFORCING STEEL | | | | LBS. | 1266 | |
| * EPOXY COATED REINFORCING STEEL | | | | LBS. | 926 | |
| CLASS AA CONCRETE | | | | C. Y. | 17.7 | |

SPLICE LENGTHS

| BAR SIZE | EPOXY COATED | UNCOATED |
|----------|--------------|----------|
| #4 | 2'-0" | 1'-9" |
| #5 | 2'-6" | 2'-2" |
| #6 | 3'-10" | 2'-7" |

PROJECT NO. **B-5800**
STANLY COUNTY
STATION: **14+88.00 -L-**



STV ENGINEERS, INC.
900 West Trade St., Suite 715
Charlotte, NC 28202
NC License Number F-0991

DOCUMENT NOT CONSIDERED
FINAL UNLESS ALL
SIGNATURES COMPLETED

| STATE OF NORTH CAROLINA DEPARTMENT OF TRANSPORTATION RALEIGH STANDARD BRIDGE APPROACH SLAB FOR PRESTRESSED CONCRETE CORED SLAB UNIT (SUB-REGIONAL TIER) 90° SKEW | | | | | SHEET NO. S-13 |
|--|-----|-------|-----|-----|--------------------|
| REVISIONS | | | | | TOTAL SHEETS 13 |
| NO. | BY: | DATE: | NO. | BY: | DATE: |
| 1 | | | 3 | | |
| 2 | | | 4 | | |

STD. NO. BAS_30_90S

STANDARD NOTES

DESIGN DATA:

| | | |
|--|-----------|-------------------------|
| SPECIFICATIONS | - - - - - | A.A.S.H.T.O. (CURRENT) |
| LIVE LOAD | - - - - - | SEE PLANS |
| IMPACT ALLOWANCE | - - - - - | SEE A.A.S.H.T.O. |
| STRESS IN EXTREME FIBER OF | | |
| STRUCTURAL STEEL - AASHTO M270 GRADE 36 | - | 20,000 LBS. PER SQ. IN. |
| - AASHTO M270 GRADE 50W | - | 27,000 LBS. PER SQ. IN. |
| - AASHTO M270 GRADE 50 | - | 27,000 LBS. PER SQ. IN. |
| REINFORCING STEEL IN TENSION | | |
| GRADE 60 | - - | 24,000 LBS. PER SQ. IN. |
| CONCRETE IN COMPRESSION | - - - - - | 1,200 LBS. PER SQ. IN. |
| CONCRETE IN SHEAR | - - - - - | SEE A.A.S.H.T.O. |
| STRUCTURAL TIMBER - TREATED OR | | |
| UNTREATED - EXTREME FIBER STRESS | - - - - - | 1,800 LBS. PER SQ. IN. |
| COMPRESSION PERPENDICULAR TO GRAIN OF TIMBER | - - - - | 375 LBS. PER SQ. IN. |
| EQUIVALENT FLUID PRESSURE OF EARTH | - - - - - | 30 LBS. PER CU. FT. |
| | | (MINIMUM) |

MATERIAL AND WORKMANSHIP:

EXCEPT AS MAY OTHERWISE BE SPECIFIED ON PLANS OR IN THE SPECIAL PROVISIONS, ALL MATERIAL AND WORKMANSHIP SHALL BE IN ACCORDANCE WITH THE 2012 "STANDARD SPECIFICATIONS FOR ROADS AND STRUCTURES" OF THE N. C. DEPARTMENT OF TRANSPORTATION.

STEEL SHEET PILING FOR PERMANENT OR TEMPORARY APPLICATIONS SHALL BE HOT ROLLED.

CONCRETE:

UNLESS OTHERWISE REQUIRED ON PLANS, CLASS A CONCRETE SHALL BE USED FOR ALL PORTIONS OF ALL STRUCTURES WITH THE EXCEPTION THAT: CLASS AA CONCRETE SHALL BE USED IN BRIDGE SUPERSTRUCTURES, ABUTMENT BACKWALLS, AND APPROACH SLABS; AND CLASS B CONCRETE SHALL BE USED FOR SLOPE PROTECTION AND RIP RAP.

CONCRETE CHAMFERS:

UNLESS OTHERWISE NOTED ON THE PLANS, ALL EXPOSED CORNERS ON STRUCTURES SHALL BE CHAMFERED 3/4" WITH THE FOLLOWING EXCEPTIONS: TOP CORNERS OF CURBS MAY BE ROUNDED TO 1-1/2" RADIUS WHICH IS BUILT INTO CURB FORMS; CORNERS OF TRANSVERSE FLOOR EXPANSION JOINTS SHALL BE ROUNDED WITH A 1/4" FINISHING TOOL UNLESS OTHERWISE REQUIRED ON PLANS; AND CORNERS OF EXPANSION JOINTS IN THE ROADWAY FACES AND TOPS OF CURBS AND SIDEWALKS SHALL BE ROUNDED TO A 1/4" RADIUS WITH A FINISHING STONE OR TOOL UNLESS OTHERWISE REQUIRED ON PLANS.

DOWELS:

DOWELS WHEN INDICATED ON PLANS AS FOR CULVERT EXTENSIONS, SHALL BE EMBEDDED AT LEAST 12" INTO THE OLD CONCRETE AND GROUTED INTO PLACE WITH 1:2 CEMENT MORTAR.

ALLOWANCE FOR DEAD LOAD DEFLECTION, SETTLEMENT, ETC. IN CASTING SUPERSTRUCTURES:

BRIDGES SHALL BE BUILT ON THE GRADE OR VERTICAL CURVE SHOWN ON PLANS. SLABS, CURBS AND PARAPETS SHALL CONFORM TO THE GRADE OR CURVE. ALL DIMENSIONS WHICH ARE GIVEN IN SECTION AND ARE AFFECTED BY DEAD LOAD DEFLECTIONS ARE DIMENSIONS AT CENTER LINE OF BEARING UNLESS OTHERWISE NOTED ON PLANS. IN SETTING FORMS FOR STEEL BEAM BRIDGES AND PRESTRESSED CONCRETE GIRDER BRIDGES, ADJUSTMENTS SHALL BE MADE DUE TO THE DEAD LOAD DEFLECTIONS FOR THE ELEVATIONS SHOWN. WHERE BLOCKS ARE SHOWN OVER BEAMS FOR BUILDING UP TO THE SLAB, THE VERTICAL DIMENSIONS OF THE BLOCKS SHALL BE ADJUSTED BETWEEN BEARINGS TO COMPENSATE FOR DEAD LOAD DEFLECTIONS, VERTICAL CURVE ORDINATE, AND ACTUAL BEAM CAMBER. WHERE BOTTOM OF SLAB IS IN LINE WITH BOTTOM OF TOP FLANGES, DEPTH OF SLAB BETWEEN BEARINGS SHALL BE ADJUSTED TO COMPENSATE FOR DEAD LOAD DEFLECTION, VERTICAL CURVE ORDINATE, AND ACTUAL BEAM CAMBER. IN SETTING FALSEWORK AND FORMS FOR REINFORCED CONCRETE SPANS, AN ALLOWANCE SHALL BE MADE FOR DEAD LOAD DEFLECTIONS, SETTLEMENT OF FALSEWORK, AND PERMANENT CAMBER WHICH SHALL BE PROVIDED FOR IN ADDITION TO THE ELEVATIONS SHOWN. AFTER REMOVAL OF THE FALSEWORK, THE FINISHED STRUCTURES SHALL CONFORM TO THE PROFILE AND ELEVATIONS SHOWN ON THE PLANS AND CONSTRUCTION ELEVATIONS FURNISHED BY THE ENGINEER. DETAILED DRAWINGS FOR FALSEWORK OR FORMS FOR BRIDGE SUPERSTRUCTURE AND ANY STRUCTURE OR PARTS OF A STRUCTURE AS NOTED ON THE PLANS SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL BEFORE CONSTRUCTION OF THE FALSEWORK OR FORMS IS STARTED.

REINFORCING STEEL:

ALL REINFORCING STEEL SHALL BE DEFORMED. DIMENSIONS RELATIVE TO PLACEMENT OF REINFORCING ARE TO CENTERS OF BARS UNLESS OTHERWISE INDICATED IN THE PLANS. DIMENSIONS ON BAR DETAILS ARE TO CENTERS OF BARS OR ARE OUT TO OUT AS INDICATED ON PLANS. WIRE BAR SUPPORTS SHALL BE PROVIDED FOR REINFORCING STEEL WHERE INDICATED ON THE PLANS. WHEN BAR SUPPORT PIECES ARE PLACED IN CONTINUOUS LINES, THEY SHALL BE SO PLACED THAT THE ENDS OF THE SUPPORTING WIRES SHALL BE LAPPED TO LOCK LEGS ON ADJOINING PIECES.

STRUCTURAL STEEL:

AT THE CONTRACTOR'S OPTION, HE MAY SUBSTITUTE 7/8"Ø SHEAR STUDS FOR THE 3/4"Ø STUDS SPECIFIED ON THE PLANS. THIS SUBSTITUTION SHALL BE MADE AT THE RATE OF 3 - 7/8"Ø STUDS FOR 4 - 3/4"Ø STUDS, AND STUD SPACING CHANGES SHALL BE MADE AS NECESSARY TO PROVIDE THE SAME EQUIVALENT NUMBER OF 7/8"Ø STUDS ALONG THE BEAM AS SHOWN FOR 3/4"Ø STUDS BASED ON THE RATIO OF 3 - 7/8"Ø STUDS FOR 4 - 3/4"Ø STUDS. STUDS OF THE LENGTH SPECIFIED ON THE PLANS MUST BE PROVIDED. THE MAXIMUM SPACING SHALL BE 2'-0". EXCEPT AT THE INTERIOR SUPPORTS OF CONTINUOUS BEAMS WHERE THE COVER PLATE IS IN CONTACT WITH BEARING PLATE, THE CONTRACTOR MAY, AT HIS OPTION, SUBSTITUTE FOR THE COVER PLATES DESIGNATED ON THE PLANS COVER PLATES OF THE EQUIVALENT AREA PROVIDED THESE PLATES ARE AT LEAST 5/16" IN THICKNESS AND DO NOT EXCEED A WIDTH EQUAL TO THE FLANGE WIDTH LESS 2" OR A THICKNESS EQUAL TO 2 TIMES THE FLANGE THICKNESS. THE SIZE OF FILLET WELDS SHALL CONFORM TO THE REQUIREMENTS OF THE CURRENT ANSI/AASHTO/AWS "BRIDGE WELDING CODE". ELECTROSLAG WELDING WILL NOT BE PERMITTED. WITH THE SOLE EXCEPTION OF EDGES AT SURFACES WHICH BEAR ON OTHER SURFACES, ALL SHARP EDGES AND ENDS OF SHAPES AND PLATES SHALL BE SLIGHTLY ROUNDED BY SUITABLE MEANS TO A RADIUS OF APPROXIMATELY 1/16 INCH OR EQUIVALENT FLAT SURFACE AT A SUITABLE ANGLE PRIOR TO PAINTING, GALVANIZING, OR METALLIZING.

HANDRAILS AND POSTS:

METAL STANDARDS AND FACES OF THE CONCRETE END POSTS FOR THE METAL RAIL SHALL BE SET NORMAL TO THE GRADE OF THE CURB, UNLESS OTHERWISE SHOWN ON PLANS. THE METAL RAIL AND TOPS OF CONCRETE POSTS USED WITH THE ALUMINUM RAIL SHALL BE BUILT PARALLEL TO THE GRADE OF THE CURB. METAL HANDRAILS SHALL BE IN ACCORDANCE WITH THE PLANS. RAILS SHALL BE AS MANUFACTURED FOR BRIDGE RAILING. CASTINGS SHALL BE OF A UNIFORM APPEARANCE. FINIS AND OTHER DEFORMATIONS RESULTING FROM CASTING OR OTHERWISE SHALL BE REMOVED IN A MANNER SO THAT A UNIFORM COLORING OF THE COMPLETED CASTING SHALL BE OBTAINED. CASTINGS WITH DISCOLORATIONS OR OF NON-UNIFORM COLORING WILL NOT BE ACCEPTED. CERTIFIED MILL REPORTS ARE REQUIRED FOR METAL RAILS AND POSTS.

SPECIAL NOTES:

GENERALLY, IN CASE OF DISCREPANCY, THIS STANDARD SHEET OF NOTES SHALL GOVERN OVER THE SPECIFICATIONS, BUT THE REMAINDER OF THE PLANS SHALL GOVERN OVER NOTES HEREON, AND SPECIAL PROVISIONS SHALL GOVERN OVER ALL. SEE SPECIFICATIONS ARTICLE 105-4.

ENGLISH

JANUARY, 1990

STD. NO. SN